

Texas-Mexico
International Bridges and Border Crossings
Existing and Proposed
2004

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Existing and Proposed
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OVERVIEW

This publication provides general information about each bridge and border crossing that connects Texas and the Mexican states of Tamaulipas, Nuevo Leon, Coahuila and Chihuahua. The bridges are listed east-to-west geographically. The total number of vehicular international bridges along the Texas-Mexico border is 23. This does not include the La Linda Bridge, which is closed, and the Roma International Suspension Bridge that is also closed and being considered for rehabilitation. The two dam crossings and one hand-drawn ferry that join Texas and Mexico bring the total number of international bridges and border crossings to 26.

In addition, five rail-only bridges span the Texas-Mexico border. They are not included in this publication because they do not require connecting highway infrastructure. These bridges are at Laredo, Eagle Pass, Presidio and two at El Paso. For more information about these railroad crossings, please contact the TxDOT Multimodal Operations Section of the Transportation Planning and Programming Division at (512) 416-2349.

There are seven proposed bridges in Texas along the U.S.-Mexican border, which are listed in Appendix I. The Port of Brownsville, Donna International Bridge, Mission International Bridge and Anzalduas International Bridge have been issued Presidential Permits while the remaining three bridges have a permit application pending or are in the permitting process. Before construction of a bridge can begin, the bridge sponsor must go through an extensive approval process to obtain the necessary permits. Refer to Appendix III for more detailed information about the permit process.

The development of permanent Border Safety Inspection Facilities is underway for sites in El Paso at the Bridge of the Americas (BOTA) and Ysleta-Zaragoza Bridge; in Eagle Pass at the Camino Real International Bridge; in Laredo at the Colombia-Solidarity and World Trade Bridges; in Pharr at the Pharr-Reynosa International Bridge on the Rise; and Brownsville at the Veterans International Bridge (Los Tomates) and at Los Indios Free Trade Bridge.

In preparation for the future opening of the border, and to address the concern about the safety of Mexican trucks, the Department of Public Safety (DPS) and the Texas Department of Transportation (TxDOT) agreed that temporary sites would be provided at each of the 8 locations mentioned. Temporary inspection sites are presently operational at those sites. DPS operates along side Federal Motor Carrier Safety Administration personnel within the confines of their facility at the World Trade Bridge in Laredo.

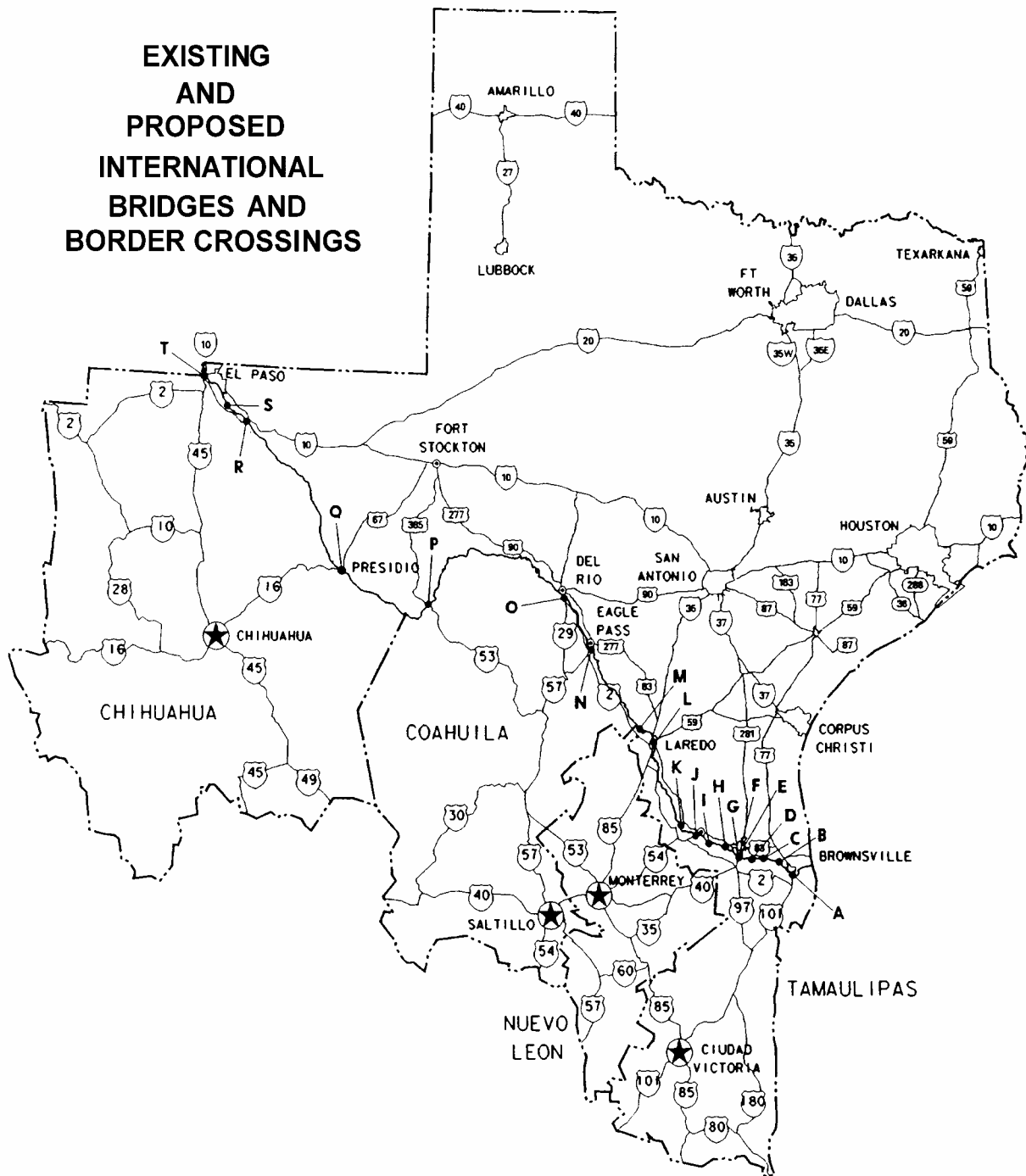
Commercial hours of operation for the borders crossings indicated herein are for Monday-Friday only. Please contact the port of entry directly for weekend and holiday hours of service. Hours of operation are subject to change. Southbound traffic information was not included due to insufficient data.

DISCLAIMER

The Texas Department of Transportation's (TxDOT) International Relations Office has compiled the data contained in this publication from various government and private sources. While we are most grateful for the information provided, the Texas Department of Transportation does not assume responsibility for the data. Readers should note we present the information as provided from the source. We have attempted to attribute the source for each entry, as well as the corresponding date of information when available. Every effort has been made to update all the information contained in the publication. However, some information may not have been updated due to lack of response from our sources. Please contact the TxDOT International Relations Office at (512) 374-5326 for questions related to this publication.

MAP

EXISTING AND PROPOSED INTERNATIONAL BRIDGES AND BORDER CROSSINGS



LEGEND

- | | |
|--|--|
| <p>A. <u>Brownsville-Matamoros</u>
Port of Brownsville Bridge (<i>Proposed</i>)
Veterans Intl. Bridge at Los Tomates
Gateway International Bridge
B & M Bridge</p> | <p>N. <u>Eagle Pass-Piedras Negras</u>
Camino Real International Bridge
Eagle Pass Bridge I</p> |
| <p>B. <u>Los Indios-Lucio Blanco</u>
Free Trade Bridge</p> | <p>O. <u>Del Rio-Ciudad Acuña</u>
Del Rio-Ciudad Acuña International Bridge
Lake Amistad Dam Crossing</p> |
| <p>C. <u>Progreso-Nuevo Progreso</u>
Progreso International Bridge</p> | <p>P. <u>La Linda</u>
La Linda Bridge (<i>Closed</i>)</p> |
| <p>D. <u>Donna-Rio Bravo</u>
Donna International Bridge (<i>Proposed</i>)</p> | <p>Q. <u>Presidio-Ojinaga</u>
Presidio Bridge</p> |
| <p>E. <u>Pharr-Reynosa</u>
Pharr-Reynosa International Bridge on the Rise</p> | <p>R. <u>Fort Hancock-El Porvenir</u>
Fort Hancock-El Porvenir Bridge</p> |
| <p>F. <u>Hidalgo-Reynosa</u>
McAllen-Hidalgo-Reynosa Bridge
Anzalduas Bridge (<i>Proposed</i>)</p> | <p>S. <u>Fabens-Caseta</u>
Fabens-Caseta Bridge
Guadalupe-Tornillo International Bridge
(<i>Proposed</i>)</p> |
| <p>G. <u>Mission-Reynosa</u>
Mission International Bridge (<i>Proposed</i>)</p> | <p>T. <u>El Paso-Ciudad Juárez</u>
Ysleta-Zaragoza Bridge
Bridge of the Americas (BOTA)
Good Neighbor Bridge
Paso del Norte Bridge</p> |
| <p>H. <u>Los Ebanos-Gustavo Diaz Ordaz</u>
Los Ebanos Ferry
Los Ebanos International Bridge
(<i>Proposed</i>)</p> | |
| <p>I. <u>Rio Grande City-Camargo</u>
Rio Grande City-Camargo Bridge</p> | |
| <p>J. <u>Roma-Ciudad Miguel Alemán</u>
Roma-Ciudad Miguel Alemán Bridge
Roma International Suspension Bridge
(<i>Closed - Rehabilitation Pending</i>)</p> | |
| <p>K. <u>Falcon Heights-Ciudad Guerrero</u>
Lake Falcon Dam Crossing</p> | |
| <p>L. <u>Laredo-Nuevo Laredo</u>
Juárez-Lincoln Bridge
Gateway to the Americas Bridge
World Trade Bridge
Laredo V International Bridge (<i>Proposed</i>)</p> | |
| <p>M. <u>Laredo-Colombia</u>
Laredo-Colombia Solidarity Bridge</p> | |

SUMMARY
Texas-Mexico Crossings
FACT SHEET

Number of operational vehicular bridges: **23***

Numbers of vehicular dam crossings: **2** - Lake Falcon Dam Crossing, Lake Amistad Dam Crossing

Number of ferries: **1** - Los Ebanos Ferry

Total number of bridges and border crossings (excluding rail-only crossings): **26***

Number of rail-only crossings: **5** - Eagle Pass, Laredo, Presidio and two at El Paso

Number of vehicular-rail crossings: **1** - B & M Bridge in Brownsville

Number of bridges under construction: **0**

Number of bridges considered for rehabilitation: **1** - Roma International Suspension Bridge

Number of proposed bridges: **7**** - Port of Brownsville, Donna International, Anzalduas International, Mission International, Los Ebanos International, Laredo V International Bridge and Guadalupe-Tornillo International Bridge

Number of bridges and crossings that are privately owned: **4** - B & M, Progreso, Los Ebanos Ferry, Rio Grande City-Camargo

Number of bridges owned by the State of Texas: **1** – Presidio Bridge

Number of bridges and crossings owned by the U.S. Federal Government: **5** - Lake Falcon Dam Crossing, Lake Amistad Dam Crossing, Fort Hancock-El Porvenir, Fabens-Caseta, and Bridge of the Americas

Number of bridges or crossings owned or operated by cities and/or counties: **16** - Veterans Intl. Bridge at Los Tomates, Gateway International, Free Trade, Pharr-Reynosa International, McAllen-Hidalgo-Reynosa, Roma-Ciudad Miguel Alemán, Juarez-Lincoln, Gateway to the Americas, World Trade Bridge, Laredo-Colombia Solidarity, Camino Real International, Eagle Pass I, Del Rio-Ciudad Acuña International, Ysleta-Zaragoza, Good Neighbor and Paso Del Norte.

Number of Texas Department of Transportation Districts along the border: **4** - Pharr, Laredo, Odessa and El Paso

Length of the Texas-Tamaulipas border: **348.6** miles

Length of the Texas-Nuevo León border: **11.8** miles

Length of the Texas-Coahuila border: **481** miles

Length of the Texas-Chihuahua border: **413.3** miles

Length of the Texas-Mexico border: 1,254.7 miles

Source: Miguel Pavón, Texas-Mexico Borderlands Information Center, Texas Water Development Board

* This number does not include the Roma International Suspension Bridge, which is currently closed and is being considered for rehabilitation, or the La Linda Bridge, which was closed on August 15, 1996.

** Proposed bridges with a Presidential Permit application submitted, approved or pending approval.

**Veterans International Bridge at Los Tomates
Brownsville, Texas – Matamoros, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Los Tomates Bridge, Expressway 77 Bridge and Brownsville Expressway Bridge, Puente Internacional Ignacio Zaragoza

LOCATION: TxDOT District: Pharr
Texas County: Cameron
U.S. City: Brownsville
Mexican City: Matamoros

DESCRIPTION: A four-lane bridge with sidewalks on both sides.
Bridge length: 4024 feet
Bridge identification number-C003-02-001
Source: Pharr District, TxDOT, August 1999

Texas Senate Bill 934 (76th Legislature-1999) expanded the limits of the “heavy truck corridor” established under Senate Bill 1276 (75th Legislature-1997) to include US 77/83 and SH 4 between the Veterans International Bridge and the Port of Brownsville. Overweight vehicles are permitted to use these routes provided they obtain permits from the Brownsville Navigation District. Detailed information on the permits is available from the Brownsville Navigation District.

Source: Pharr District, TxDOT, August 1999

U.S.: The bridge began operating on April 30, 1999 and began processing all northbound commercial traffic for the Brownsville area on May 1, 1999. The bridge began processing southbound commercial traffic on July 12, 1999. Per Diplomatic Notes between the State Department and the Secretaría de Relaciones Exteriores (SRE) on August 9, 1999, all southbound commercial traffic for this area was to be directed to the

bridge by December 30, 1999. However, there was a delay and effective March 1, 2001; all trucks (loaded/empty) in the area began to be redirected for processing to the Veterans International Bridge.

Source: Pete Sepulveda, Cameron County International Bridge System, June 2001

Source: Pharr District, TxDOT, June 2001

Mexico: The construction of a new international bridge in the Brownsville/Matamoros area was necessary because of the need to move vehicular traffic from the city to outlying areas. The heavy congestion not only impacts the roadway systems of both border communities, but also the environment.

Source: SRE, December 1996

BRIDGE OWNER OR OPERATOR:

U.S. Owner: Cameron County and City of Brownsville

U.S. Operator: Cameron County International Bridge System

Mexican Owner: Government of Mexico

Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:

U.S.: The bridge became operational in April 1999.

Source: Pharr District, TxDOT, August 1999

Mexico: The contract for the Mexican portion of bridgework was awarded in January 1998. The Government of the State of Tamaulipas obtained right of way for the bridge and its attendant structures on May 8, 1998. In accordance with the Inter-institutional Contract signed by the Secretaría de Comunicaciones y Transportes (SCT), Cameron County and the City of Brownsville, the bridge was constructed and the inaugural ribbon cutting ceremony was held on April 30, 1999.

Source: Pharr District, TxDOT, August 1999

FUNDING/COST: **U.S.:** \$19.3 million

GSA: \$16.2 million (Border Station)

Source: General Services Administration, August 1999

Mexico: The Mexican federal government financed the project.

Source: CAPUFE, January 1999

TOLL:

\$2.00 for passenger vehicles

\$.50 for pedestrians

\$3.50 per axle for commercial vehicles

Source: Pete Sepulveda, Cameron County International Bridge System, January 2004

HOURS:

6 a.m. – Midnight (POV – M-Sun)

8 a.m. – 11 p.m. (Commercial/Cargo – M-Fri)

8 a.m. – 4 p.m. (Commercial/Cargo – Sat. and Sun.)

Source: U.S. Customs and Border Protection, January 2004

BORDER STATION:

U.S.: The General Services Administration (GSA) owns the border station, which became operational on April 30, 1999.

Source: Pharr District, TxDOT, August 1999

Source: General Services Administration, August 1999

Mexico: The border station became operational on April 30, 1999.

Source: Pete Sepulveda, Cameron County International Bridge System, August 1999

CONNECTING ROADWAY:

U.S.: US 77/83 Expressway extension to the Los Tomates International Bridge. This project was done in two phases at an estimated cost of \$32.7 million and opened to traffic in March 2000.

Source: Pharr District, TxDOT, November 2000

Phase I of the extension, from SH4 to the GSA Complex at the bridge, was let in September 1997. Project cost was \$17.8 million. A mostly 4-lane elevated section, the project provides direct access from the port of entry to International Blvd. and serves as part of the Brownsville truck route. This project was completed in December 1999.

Source: Pharr District, TxDOT, November 2000

Phase II included the completion of the overpass over International Boulevard, with a direct connection to the US 77/83 Expressway. The overpasses over 14th Street as well as the on-off ramps were widened in order to carry the expected traffic. This section was let in April of 1999 at a cost of \$14.9 million. Construction began July 7, 1999 and the project was completely finished in July 2000.

Source: Pharr District, TxDOT, November 2000

Mexico: Boulevard Luis Donaldo Colossio connects to the highway leading to Ciudad Victoria and to Reynosa.

Source: Centro S.C.T. Tamaulipas, June 2001

IMPROVEMENTS:

U.S.: U.S. Customs installed a fixed-site truck x-ray system that x-rays a vehicle in approximately 10 minutes. A mobile x-ray unit has been in operation at this port in since November 1999. The outbound canopy construction is complete.

Source: Pharr District, TxDOT, January 2004

Source: Pete Sepulveda, Cameron County International Bridge System, January 2004

The County has proposed some improvements to the commercial truck lanes; however the schedule and scope have not been defined.

Source: General Services Administration, March 2004

**Gateway International Bridge
Brownsville, Texas – Matamoros, Tamaulipas**



The United States is shown on the left in the photo.

LOCAL NAMES: El Puente, Puente Nuevo and Puerta México

LOCATION: TxDOT District: Pharr
Texas County: Cameron
U.S. City: Brownsville
Mexican City: Matamoros

DESCRIPTION: There are twin structures with a total of four lanes. One structure is southbound the other is northbound.
Source: Design Division, TxDOT
Bridge length: 687 feet southbound span, 477 feet northbound span
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification numbers-21 031 C00804 001 01 southbound span, 21 031 C00804 002 01 northbound span.

The bridge stopped processing northbound commercial traffic on April 30, 1999. Southbound commercial traffic stopped processing on February 28, 2001, at which time all commercial traffic in the area began using the Veterans International Bridge, per agreement between the State Department and the Secretaría de Relaciones Exteriores (SRE).
Source: Pete Sepulveda, Cameron County International Bridge System, June 2001

BRIDGE OWNER OR OPERATOR:

U.S. Owner: Cameron County
U.S. Operator: Cameron County International Bridge System
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios

Conexos (CAPUFE)

YEAR OF CONSTRUCTION:

One span of the bridge was completed in 1969, the other in 1970.

Source: Bridge inspection report provided by José Galván, February 1995

HOURS:

24 hours (POV only)

Source: U.S. Customs and Border Protection, January 2004

TOLL:

\$2.00 for passenger vehicles

\$.50 for pedestrians

Source: Pete Sepulveda, Cameron County International Bridge System, January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

Presidential Permits were not required for bridges built before 1972.

BORDER STATION:

U.S.: The border station was completed in 1969. The renovation and expansion was completed in March 1994.

Source: General Services Administration, Summary of Existing and Proposed Border Stations, May 1995

Mexico: The border station has been operational since 1961 and was remodeled in 1968. The station is outfitted with traffic signals to use for random checks to help speed up vehicular traffic.

Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: International Boulevard (SH 4) interconnects US 83/77

Mexico: Av. Alvaro Obregón connects with MEX 2 and MEX 101/180

IMPROVEMENTS:

U.S.: Texas State Senate Bill 1276 (75th Legislature, 1997) permitted TxDOT to grant the Brownsville Navigation District (BND) the authority to issue permits for the movement of oversize or overweight vehicles carrying cargo between the Gateway International Bridge and the entrance to the Port of Brownsville. The Texas Transportation Commission on February 17, 1998 approved the BND's permit-issuing authority. By December 15, 1998, the Port of Brownsville had issued 23,713 such permits. Overweight and oversize trucks issued permits by the Port of Brownsville are restricted to a designated truck route between the Port and Bridge. Money charged for the permits is used for TxDOT maintenance costs along the designated truck route. However, since all commercial traffic has been redirected to the Veterans International Bridge, the route for overweight trucks from the Port of Brownsville has also been redirected to the Veterans International Bridge.

Source: Port of Brownsville, December 1998

Source: Pharr District, TxDOT, September 1998

Source: Pete Sepulveda, Cameron County International Bridge System, August 2002

B & M Bridge
Brownsville, Texas – Matamoros, Tamaulipas



The United States is shown at the bottom of the photo.

LOCAL NAMES: Brownsville & Matamoros Bridge, B y M (Cruce de ferrocarril y vehículos), Puente Viejo and the Express Bridge

LOCATION: TxDOT District: Pharr
Texas County: Cameron
U.S. City: Brownsville
Mexican City: Matamoros

DESCRIPTION: There are two bridges; the existing old bridge consists of a single railroad track or two commercial lanes, one in each direction. The bridge expansion consists of four lanes, two in each direction, and is used exclusively for non-commercial traffic. The southbound pedestrian walkway is located on the old bridge while the northbound pedestrian walkway is located on the new bridge expansion.

Source: Pharr District, TxDOT, November 2000

The bridge stopped processing northbound commercial traffic on April 30, 1999. Per agreement between the State Department and the Secretaría de Relaciones Exteriores (SRE), southbound commercial traffic was processed only until December 30, 1999, at which time all commercial traffic began to use the Veterans International Bridge at Los Tomates.

Source: Secretaría de Relaciones Exteriores, November 2000

Source: Brownsville & Matamoros Bridge Company, June 2001

BRIDGE OWNER OR OPERATOR:

The U.S. and Mexican sides of the bridge are owned and operated by the Brownsville & Matamoros Bridge Company, a subsidiary of the Union Pacific Railroad and the Federal Government of Mexico.

YEAR OF CONSTRUCTION:

The original bridge was constructed in 1909 and reconstructed in 1953. The \$5 million bridge expansion was inaugurated on May 14, 1997.

Source: Centro S.C.T. Tamaulipas, June 2001

HOURS:

24 hours (POV only)

Source: U.S. Customs and Border Protection, January 2004

TOLL:

\$2.00 – Southbound

\$2.00 – Northbound

Source: B&M Bridge Co., January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits were not required for bridges built before 1972.

BRIDGE CONSTRUCTION STATUS:

U.S.: Construction on the bridge expansion began November 1995 and was completed on May 14, 1997.

Source: Office of Bridge Administration, U.S. Coast Guard, May 1998

BORDER STATION:

U.S.: The new border station was completed in 1992, and is owned by the General Services Administration.

Source: B&M Bridge Co., June 1997

Due to the transfer of all commercial traffic to the Veteran's International Bridge, the truck import lot and dock are now used for other inspectional purposes.

Source: General Services Administration, March 2004

Mexico: The border station facility for tourist traffic was completed on May 14, 1997. The Brownsville & Matamoros Bridge Company owns all of the facilities.

Source: SEDICOT, Gobierno del Estado de Tamaulipas, July 1997

Source: B&M Bridge Co., March 2000

CONNECTING ROADWAY:

U.S.: 12th Street and Mexico Street interconnect US B77

Mexico: Las Americas Avenue via MEX-101.

Source: SEDICOT, Gobierno del Estado de Tamaulipas, July 1997

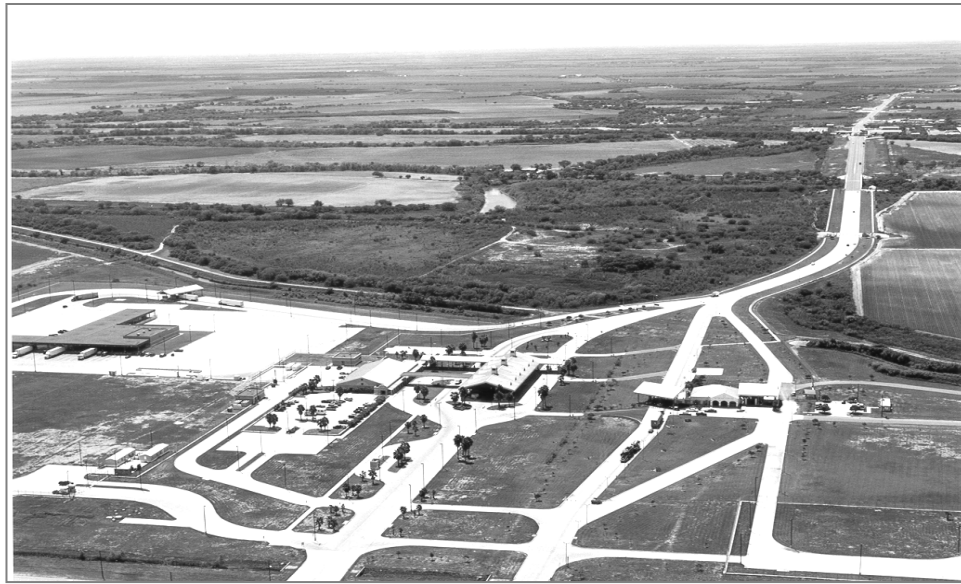
IMPROVEMENTS:

A SENTRI project using the old railroad truss bridge as a Dedicated Commuter Lane (DCL) is being considered. The DCL would include the use of a pre-paid card or transponder that would allow users a faster processing time. The old railroad truss bridge is under general maintenance to be stripped and re-painted for this purpose.

Source: B&M Bridge Co., January 2004

Source: Pharr District, TxDOT, February 2004

**Free Trade Bridge
Los Indios, Texas – Lucio Blanco, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Los Indios-Lucio Blanco Bridge, Puente Lucio Blanco-Los Indios, Puente Internacional Libre Comercio and Los Indios Free Trade Bridge

LOCATION: TxDOT District: Pharr
Texas County: Cameron
U.S. City: Los Indios
Mexican City: Lucio Blanco

DESCRIPTION: A four-lane bridge
Bridge length: 503 feet
Source: Bridge Division, TxDOT, June 2001
Bridge identification number-21 031 N00926 001 01

BRIDGE OWNER OR OPERATOR:

U.S. Owners: Cameron County (50%), City of San Benito (25%), and City of Harlingen (25%)

U.S. Operator: Cameron County International Bridge System

Mexican Owner: Government of Mexico. The State of Tamaulipas has the concession until 2008.

Source: Programa Nacional de Autopistas 1989-1994, *Propósitos y Logros*, Secretaría de Comunicaciones y Transportes

Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:

Completed on November 2, 1992.

FUNDING/COST: U.S.: \$31.6 million

HOURS: 6 a.m. – Midnight (POV – M-Sun)
8 a.m. – 10 p.m. (Commercial/Cargo – M-Fri)
10 a.m. – 6 p.m. (Commercial/Cargo – Sat. and Sun.)
Source: U.S. Customs and Border Protection, January 2004

TOLL: \$2.00 for passenger vehicles
\$.50 for pedestrians
\$3.50 per axle for commercial vehicles
Source: Pete Sepulveda, Cameron County International Bridge System, January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Cameron County and the cities of San Benito and Harlingen submitted a Presidential Permit application January 18, 1974, which was approved November 30, 1990.

USCG approved a bridge permit on April 12, 1991.
Source: Nick Mpras, USCG

BORDER STATION:

U.S.: The border station was completed on November 2, 1992, and is owned by the General Services Administration.

Mexico: The border station has been operational since November 1992.
Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: FM 509, from US 83/77 Expressway South to GSA facilities at Los Indios Free Trade Bridge; Length: 8.3 miles; Cost: \$7.8 million.
Source: Pharr District, TxDOT, June 1997

IMPROVEMENTS:

U.S.: Foreign Trade Zone was granted by the U.S. Department of Commerce to the Free Trade Commerce Center, a 750 acre industrial park located adjacent to the bridge.

Source: Pete Sepulveda, Cameron County International Bridge System, January 2004
Source: Pharr District, TxDOT, January 2004

Mexico: The development of an industrial and residential zone in Mexico is in the preliminary planning phase.

Source: Pete Sepulveda, Cameron County International Bridge System, January 2004

**Progreso International Bridge
Progreso, Texas – Nuevo Progreso, Tamaulipas**



**The United States is shown at the bottom of the photo.
(Photo taken in 2000 before bridge replacement)**

LOCAL NAMES: B & P Bridge, Puente Las Flores and Puente Internacional Nuevo Progreso-Progreso

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Progreso
Mexican City: Nuevo Progreso

DESCRIPTION: A new four-lane automobile bridge, two lanes in each direction, plus a separate new two-lane truck bridge.

Bridge length: 628 feet

Bridge identification number-21 109 V00455 001 01

Source: Bridge Inventory and Inspection File, TxDOT

Source: B & P Bridge Company, January 2004

BRIDGE OWNER OR OPERATOR:

U.S. Owner: B & P Bridge Company of Weslaco

Mexican Owner: Government of Mexico

Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:

The original bridge was constructed in 1951. The replacement bridge was built in phases as the old one was simultaneously demolished. The replacement bridge became operational in August 2003.

Source: B & P Bridge Company, January 2004

FUNDING/COST: U.S.: Private funds were used for construction of this bridge.

HOURS: 24 hours (POV)
8 a.m. – 6 p.m. (Commercial/Cargo – M-Fri)
Source: B & P Bridge Company, January 2004

TOLL: \$1.75
Source: B & P Bridge Company, January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits were not required for bridges built before 1972. The State Department determined that the owner did not need to secure a Presidential Permit to build the replacement bridge. The replacement bridge became operational in August 2003.

Source: U.S. State Department, February 1995

Source: B & P Bridge Company, January 2004

A Coast Guard bridge permit (7-00-8) approving the location and plans of the replacement project was issued on March 20, 2000 to the B & P Bridge Company.

Source: Office of Bridge Administration, U.S. Coast Guard, March 2000

Mexico: The Government of the State of Tamaulipas has taken steps to forward project plans and designs and obtain the necessary approval from CILA, CAPUFE, and the SCT.

Source: SEDICOT, Gobierno del Estado de Tamaulipas, May 1998

BORDER STATION:

U.S.: The existing border station facility was completed in 1983. The B&P Bridge Company owns the U.S. border station facility. The border station was expanded and became fully operational at the time the replacement bridge opened in August 2003. The GSA is planning an expansion of the inspection docks facilities to be completed by mid-2004.

Source: Gibson Gonzalez Associates, October 2002

Source: B & P Bridge Company, January 2004

Source: Pharr District, TxDOT, January 2004

A gamma-ray state of the art machine is operational at the import lot, and has reduced the number of examination docks needed in the future.

Source: Pharr District, TxDOT, June 2001

Source: Centro S.C.T. Tamaulipas, June 2001

The import lot expansion included the installation of x-ray equipment. The U.S. border station receives all types of cargo entering from Mexico.

Source: General Services Administration, June 2001

Mexico: The border station has been in operation since 1951.

Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: FM 1015, connects to US 281 and to US 83

Mexico: Carretera Estatal Nuevo Progreso connects to MEX 2

IMPROVEMENTS:

U.S.: The first phase of the new four-lane bridge was completed by November 17, 2002. The transfer of traffic from the old bridge to the new bridge became effective on November 27, 2002. The new bridge, which includes four automobile lanes and pedestrian sidewalks, became operational in August 2003.

Source: B & P Bridge Company, January 2004

Facilities for truck inspections by the Federal Motor Carrier Safety Administration will be constructed adjacent to the import lot.

Source: General Services Administration, March 2004

Mexico: The opening of the new two-lane truck bridge is pending agreements with Mexico and the building of the import lot on the Mexican side. The land has been acquired and is ready for the temporary or final facilities to be built.

Source: B & P Bridge Company, January 2004

Source: Pharr District, TxDOT, January 2004

**Pharr-Reynosa International Bridge on the Rise
Pharr, Texas – Reynosa, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAME: Puente Internacional Reynosa-Pharr and Nuevo Amanecer

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Pharr
Mexican City: Reynosa

DESCRIPTION:

This is a four-lane bridge with a pedestrian walkway on one side.

Bridge length: 15,770 feet

Bridge identification number-21-109-R005-65-001

Source: Bridge Division, TxDOT, June 2001

Beginning September 1, 1996, all northbound commercial traffic was directed from the Hidalgo-Reynosa Bridge to the Pharr Bridge. Southbound commercial traffic was permitted to use either the Hidalgo or Pharr Bridge to return to Mexico.

Source: U.S. Customs and Border Protection, August 1996

BRIDGE OWNER OR OPERATOR:

U.S.: City of Pharr

Mexican Owner: Government of Mexico

Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios
Conexos (CAPUFE)

Source: S.C.T., December 1998

YEAR OF CONSTRUCTION:

Completed in November 1994 and opened on January 10, 1995.

Source: Pharr District, TxDOT

FUNDING/COST: U.S.: Bridge cost was \$18 million.

Source: Ernesto S. Silva, Interim Pharr City Manager, December 1996

The General Services Administration facilities cost \$17.6 million.

Source: *Summary of Existing and Proposed Border Stations*, General Services Administration, November 1996

HOURS: 6 a.m. – Midnight (POV – M-Sun)

6 a.m. – 10 p.m. (Commercial/Cargo – M-Fri)

Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes, an automated toll collection system is used.

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The City of Pharr submitted a Presidential Permit application, which was approved December 20, 1978. The United States Coast Guard bridge permit was approved December 10, 1991.

Source: United States Coast Guard

BORDER STATION:

U.S.: The permanent facilities were opened to traffic in April 1996 and are equipped with a fixed-site truck x-ray system that became operational in October 1997. The facility was expanded to allow for a USDA annex, kennel building, and an expanded dock. The expansion project was completed in late 2002.

Source: Pharr-Reynosa International Bridge, January 2004

The GSA facilities were upgraded by adding a port-of-entry toll collection system with the aid of a Federal Corridor Border Infrastructure grant. The project was let for approximately \$798,602 and is complete.

Source: Pharr District, TxDOT, January 2004

Mexico: The station is outfitted with traffic signals for use in random checks to help speed up vehicular traffic.

Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: Spur 600, a six-lane structure, connects from US 281 to the GSA facilities.

Source: Pharr District, TxDOT, February 1997

Mexico: A loop connects with MEX 2 and MEX 40.

Source: SEDICOT, Gobierno del Estado de Tamaulipas, February 1997

IMPROVEMENTS:

U.S.: US 281/SP 241 overpass. The project was let in March 2001 at a cost of \$6.6 million and was completed in February 2003. This overpass (US 281 over SP 241) will improve safety and circulation by separating international traffic from local east west through traffic.

Source: Pharr District, TxDOT, January 2004

The Hidalgo County MPO received funding from the Transportation Equity Act (TEA-21) for the construction of the additional import dock space, which was completed in late 2002.

Source: Pharr-Reynosa International Bridge, January 2004

Source: General Services Administration, March 2004

Mexico: The Mexico bypass around Reynosa that connects the Reynosa-Matamoros and the Reynosa-Monterrey highways is complete. The bypass will also connect to the Avenida Puente Pharr.

Source: Pharr District, TxDOT, January 2004

**McAllen-Hidalgo-Reynosa Bridge
Hidalgo, Texas – Reynosa, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Hidalgo Bridge, Puente Reynosa and Puente Reynosa-McAllen I

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Hidalgo
Mexican City: Reynosa

DESCRIPTION: Two structures. The old four-lane bridge serves only southbound traffic. The new four-lane bridge serves only northbound traffic.
Bridge length: 524 feet southbound span, 852 feet northbound span
Bridge identification numbers-21 109 G00090 001 01 southbound span, 21 109 G00090 002 01 northbound span.

Source: Bridge Inventory and Inspection File, TxDOT

Beginning September 1, 1996, all northbound commercial traffic was directed from the Hidalgo-Reynosa Bridge to the Pharr Bridge. Southbound commercial traffic was permitted to use either the Hidalgo or Pharr Bridge to enter Mexico.

Source: U.S. Customs and Border Protection, August 1996

BRIDGE OWNER OR OPERATOR:

U.S.: City of McAllen
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:

The first four-lane bridge was built in 1965 replacing a two-lane suspension bridge. The second four-lane bridge was built in 1987.

HOURS:

24 hours (POV only)

Source: U.S. Customs and Border Protection, January 2004

TOLL:

\$1.50 (southbound)

Source: Pharr District, TxDOT, June 2001

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The City of McAllen submitted a Presidential Permit application, which was approved in August 1985.

BORDER STATION:

U.S.: The City of McAllen owns the border station, which was completed in 1982. The General Services Administration's lease expires April 27, 2004.

Source: General Services Administration, August 2002

The design for border station improvements to include an enforcement facility with canopy and expansion of the southbound toll plaza to include 3 additional southbound lanes is complete and is estimated to cost \$2.35 million. If approved for SAFETEA funding at a future date, 80 per cent will come from federal funds and 20 per cent will come from the City of McAllen.

Source: General Services Administration, March 2004

Mexico: The border station, which has been in operation since 1965 was remodeled in 1988.

Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: Spur 600 from the bridge connects to Spur 241 which connects to US 281 and SH 336. Spur 115 connects from US 281 to US 83 Expressway.

Mexico: Near MEX 2, MEX 97 and MEX 40

IMPROVEMENTS:

U.S.: A new southbound inspection facility with canopy will be breaking ground in February 2004 at an estimated cost of \$900,000. Renovation to the administrative building at an estimated cost of \$600,000 is scheduled to begin construction in March 2004. A new parking lot will be included in the improvements.

The proposed dedicated northbound commuter lane, Secure Electronic Network for Travelers' Rapid Inspection (SENTRI) has been canceled.

Source: Pharr District, TxDOT, February 2004

Los Ebanos Ferry
Los Ebanos, Texas – Gustavo Díaz Ordaz, Tamaulipas



The United States is shown at the bottom of the photo.

LOCAL NAMES: Los Ebanos-San Miguel Camargo, Ferry Gustavo Díaz Ordaz, and Ferry Díaz Ordaz-Los Ebanos

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Los Ebanos
Mexican City: Gustavo Díaz Ordaz

DESCRIPTION: The ferry can carry passengers and vehicles. However, it cannot accommodate more than three automobiles at a time.

FERRY OWNER OR OPERATOR:
U.S.: Reyna Family
Mexican Owner: Armando De La Garza

YEAR OF CONSTRUCTION: Although the crossing has been in operation since the 1950s, the current ferry has been operating since 1979.
Source: Ed Reyna, March 1999

HOURS: 8 a.m. - 4 p.m. (POV only – M-Sun)
Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes

BORDER STATION:

The U.S. border station was completed in April 1992 and is owned by the Immigration and Naturalization Service. The bridge owner owns the land.
Source: General Services Administration, Summary of Existing and Proposed Border Stations, May 1995

CONNECTING ROADWAY:

U.S.: Near FM 886 to US 83.

Mexico: Near MEX 2

Rio Grande City-Camargo Bridge
Rio Grande City, Texas – Camargo, Tamaulipas



The United States is shown at the bottom of the photo.

LOCAL NAMES: Starr-Camargo Bridge and Puente Camargo

LOCATION: TxDOT District: Pharr
Texas County: Starr
U.S. City: Rio Grande City
Mexican City: Camargo

DESCRIPTION: A narrow two-lane bridge.
Bridge length: 591 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-21 214 C00045 001 01

BRIDGE OWNER OR OPERATOR:
U.S.: Starr-Camargo Bridge Company
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios
Conexos (CAPUFE)

YEAR OF CONSTRUCTION:
Opened in 1966
Source: Sam F. Vale, President, Starr-Camargo Bridge Company, May 1995

HOURS: 7 a.m. – Midnight (POV – M-Sun)
8 a.m. – 8 p.m. (Commercial/Cargo – M-Fri)
8 a.m. – 1 p.m. (Commercial/Cargo – Sat. and Sun.)
Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for bridges built before 1972. The permit authority for this facility was issued on September 21, 1959, pursuant to Public Law 86-343, 86th Congress, H.R. 8694.

Source: Starr-Camargo Bridge Company, May 1995

BORDER STATION:

U.S.: The original border station was constructed in 1969 by the Starr Camargo Bridge Company. A new, fully functional border station complete with primary and secondary inspection facilities, truck inspection facilities, fully concrete surfaced import lot, dock inspection facilities and VACIS gamma ray inspection system for commercial traffic is now operational. The facility was constructed for the General Services Administration, after a new lease was signed in April 1999. A groundbreaking ceremony was held in July 1999 and construction began in September 1999.

The new facility increased the border station space from 2,500 to approximately 8,000 square feet and added a 10 truck loading dock that is expandable to 15 docks. Northbound truck traffic now has access to new truck inspection booths as well as a new VACIS gamma ray inspection facility. The new lease included the construction of two tollbooths and was built on approximately 8 acres. Construction of the first permanent state-of-the-art USDOT truck inspection facility in the southwest was completed adjacent to the Rio Grande City port of entry in the summer of 2003.

Negotiations are being finalized for the construction of two additional southbound vehicular lanes; that will allow the two existing lanes to be used strictly for northbound traffic.

Source: Starr-Camargo Bridge Company, February 2004

Source: Pharr District, TxDOT, February 2004

Mexico: The present border station has been in operation since 1968. In November of 2003 the Mexican Government expropriated approximately 8 hectares of land for the expansion of the Mexican border station in Camargo, Tamaulipas. There are plans to remodel the existing facility and incorporate it into a state of the art facility that is now recognized by the Mexican Government as one of its top 25 ports of entry.

Source: Starr-Camargo Bridge Company, February 2004

CONNECTING ROADWAY:

U.S.: FM 755 to US 83 and US 281

Mexico: Carretera Puente Internacional connects to MEX 2, MEX 54 and MEX 40

**Roma-Ciudad Miguel Aleman Bridge
Roma, Texas – Ciudad Miguel Alemán, Tamaulipas**



The United States is shown at the bottom of the photo. The Roma International Suspension Bridge (closed) is shown to the left of Roma-Ciudad Miguel Aléman Bridge.

LOCAL NAMES: Starr County International Bridge, Roma Bridge and Puente Roma-Miguel Alemán

LOCATION: TxDOT District: Pharr
Texas County: Starr
U.S. City: Roma
Mexican City: Ciudad Miguel Alemán

DESCRIPTION: A two-lane bridge.
Bridge length: 810 feet
Source: Bridge Division, TxDOT, June 2001
Bridge identification number-21 214 D000 20 001 01

BRIDGE OWNER OR OPERATOR:
U.S.: Starr County
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION: 1979

HOURS: 24 hours (POV)
10 a.m. – 6 p.m. (Commercial/Cargo – M-Fri)
Source: U.S. Customs and Border Protection, January 2004

TOLL: POVs - \$2.00
Pedestrians - \$0.50
Source: Starr County International Bridge System, February 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Starr County submitted a Presidential Permit application, which was approved on April 6, 1977.

Source: Pharr District, TxDOT

BORDER STATION:

U.S.: Starr County owns the border station, which was completed in 1988. The General Services Administration's lease expires April 28, 2009.

Source: General Services Administration, Summary of Existing and Proposed Border Stations, May 1995

Mexico: There are plans to expand and modernize the Customs facilities.

Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: Spur 200 from the bridge connects to US 83

Mexico: Near MEX 2

IMPROVEMENTS:

U.S.: US 83 loop. A feasibility study to construct a four-lane divided rural section loop around the city as well as construct a pair of one-way streets along the existing US 83 and Garcia Street was completed in January 1999. The loop will start just east of Rio Grande City and go to just north of the City of Roma. Proposed letting date for the pair of one-way streets is August 2003, at an estimated cost of \$2.5 million.

Source: Pharr District, TxDOT, June 2001

Project for improvements to US 83 from Garcia Street to Loma Blanca was let in January 2003 for about \$5 million and is 64% complete. This project will provide a 4-lane divided urban facility with curb and gutter throughout this corridor. Completion is scheduled for June 2004.

Source: Pharr District, TxDOT, January 2004

**Lake Falcon Dam Crossing
Falcon Heights, Texas – Ciudad Guerrero, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Falcon Dam, Puente San Juan, Presa Falcón, Puente Internacional de la Presa

LOCATION: TxDOT District: Pharr
Texas County: Starr
U.S. City: Falcon Heights
Mexican City: Ciudad Guerrero

DESCRIPTION: A two-lane road runs above the dam.
Source: International Boundary and Water Commission

DAM OWNER OR OPERATOR:
U.S.: U.S. Section, International Boundary and Water Commission
Mexico: Mexican Section, International Boundary and Water Commission
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, February 1997

PORT-OF-ENTRY OWNERS AND OPERATORS:
U.S: U.S. Customs
Mexico: Mexican Customs
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, February 1997

YEAR OF CONSTRUCTION:
1953
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

HOURS: 7 a.m. - 9 p.m. (POV only)
Source: U.S. Customs and Border Protection, January 2004

TOLL: No

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for structures built before 1972.

BORDER STATION:

The U.S. border station was constructed in 1960. The building was expanded in 1977 and 1989. The border station was transferred from the IBWC to U.S. Customs after construction of the dam.

Source: General Services Administration, Summary of Existing and Proposed Border Stations, May 1995

U.S. Customs and Border Protection has proposed some enhancements to the facilities. GSA has been tasked with completing a feasibility study to determine the most efficient and cost effective solution of this port of entry.

Source: General Services Administration, March 2004

CONNECTING ROADWAY:

U.S.: FM 2098 to US 83

Mexico: MEX 2

**Juárez-Lincoln Bridge
Laredo, Texas – Nuevo Laredo, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Bridge #2, Laredo-Nuevo Laredo Bridge 2, Puente Juárez-Lincoln and Laredo II

LOCATION: TxDOT District: Laredo
Texas County: Webb
U.S. City: Laredo
Mexican City: Nuevo Laredo

DESCRIPTION: A six-lane bridge
Bridge length: 1008 feet
Source: Bridge Division, TxDOT, June 2001
Bridge identification number-22 240 B01060 001 01

BRIDGE OWNER OR OPERATOR:
U.S. Owner: City of Laredo
U.S. Operator: Laredo Bridge System
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:
Became operational on November 26, 1976.

FUNDING/COST: The City of Laredo financed the estimated \$8 million cost through revenue bonds. Approximately \$2 million in bonds were issued in October 1974 and \$6 million were issued in April 1980.

Source: City of Laredo, May 1995

HOURS: 24 hours (POV only)

Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Built using permit of bridge #1 (Circa 1950s).

Source: Laredo District, TxDOT

BORDER STATION:

U.S.: The General Services Administration owns the U.S. border station, which was completed in 1982. The import lot was modernized in 1993.

Source: General Services Administration, Summary of Existing and Proposed Border Stations, May 1995

Mexico: The border station became operational in November 1976.

Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: IH-35, near US 83 (Matamoros St. and Houston St.) IH-35 connects with US 59 and Loop 20. US 83 connects with Loop 20 and SH 359.

Mexico: Near MEX 85 and MEX 2

Source: Laredo District, TxDOT, June 2001

IMPROVEMENTS:

U.S.: IH-35 frontage road (southbound) reconstruction from Scott Street to Del Mar Blvd. is complete at an estimated cost of \$8.5 million. The IH-35 frontage road (northbound) was completed in November 2002 at an estimated cost of \$4.98 million.

Source: Laredo District, TxDOT, January 2004

**Gateway to the Americas Bridge
Laredo, Texas – Nuevo Laredo, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Convent Street Bridge, Laredo International Bridge, Bridge #1, Old Bridge, Laredo-Nuevo Laredo Bridge 1, Puente Nuevo Laredo, Puente Laredo I and Puente Viejo

LOCATION: TxDOT District: Laredo
Texas County: Webb
U.S. City: Laredo
Mexican City: Nuevo Laredo

DESCRIPTION: A four-lane bridge.
Bridge length: 1,050 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-22 240 B00250 001 01

BRIDGE OWNER OR OPERATOR:
U.S. Owner: City of Laredo (Laredo Bridge System)
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:
The original bridge was destroyed by flood in 1954 and reconstructed in 1956.
Source: City of Laredo, May 1995

FUNDING/COST: U.S.: This bridge was originally purchased from a private owner in 1946 for \$695,000. In 1954 floods resulting from a hurricane in the Gulf of Mexico destroyed the bridge. The city's flood damage insurance paid the bulk of the reconstruction cost of the U.S. side of the bridge. Records do not reflect the amount. The City of Laredo financed \$300,000 of the reconstruction amount (a portion not covered by insurance) through revenue bonds.

Source: City of Laredo, May 1995

HOURS: 24 hours (POV only)

Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for bridges built before 1972.

BORDER STATION:

U.S.: The General Services Administration owns the border station, which was constructed in 1943 and renovated in 1990-91.

Source: General Services Administration, August 2002

Mexico: The border station was constructed in 1954 and renovated in 1956.

Source: Centro S.C.T. Tamaulipas, June 2001

CONNECTING ROADWAY:

U.S.: BI-35 (Convent St. and Salinas St.) near IH-35 and US 83 (Matamoros St. and Houston St.)

Source: Laredo District, TxDOT, June 2001

Mexico: Near MEX 2, MEX 85 or MEX 1

IMPROVEMENTS:

The GSA is working with the City of Laredo to convert one southbound lane for use as a northbound lane, in order to allow the conversion of one northbound vehicular lane for pedestrian processing.

Source: General Services Administration, March 2004

**World Trade Bridge
Laredo, Texas – Nuevo Laredo, Tamaulipas**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Laredo North, Bridge 4, Laredo IV, Puente Internacional Nuevo Laredo III, and Puente del Comercio Mundial Nuevo Laredo III

LOCATION: TxDOT District: Laredo
Texas County: Webb
U.S. City: Laredo
Mexican City: Nuevo Laredo

DESCRIPTION: The eight-lane bridge is for commercial traffic only.
Bridge length: 977 feet
Bridge Identification Number-22-240-3483-01-025
Source: Bridge Division, TxDOT, June 2001

BRIDGE OWNER OR OPERATOR:
U.S.: City of Laredo
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios
Conexos (CAPUFE)

FUNDING/COST: U.S.: The International Bridge let for approximately \$2.2 million.
Estimates for the GSA facilities totaled over \$19.5 million. The roadway
improvement costs related to the bridge were approximately \$93 million.
Source: Laredo District, TxDOT, March 2000

HOURS: 8 a.m. – Midnight (Commercial/Cargo only – M-Fri)
8 a.m. – 4 p.m. (Commercial/Cargo – Sat.)
10 a.m. – 2 p.m. (Commercial/Cargo – Sun.)
Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The City of Laredo submitted a Presidential Permit application in 1991; the permit was issued in November 1994.

Source: Office of Mexican Affairs, U.S. Department of State, February 1997

USCG Bridge permit was approved on February 12, 1996.

Source: United States Coast Guard

A FONSI was issued by the FHWA on March 26, 1998.

Source: Laredo District, TxDOT, December 1998

Mexico: The exchange of diplomatic notes committing both nations to the construction of the new crossing took place on March 10, 1998.

BORDER STATION:

U.S.: The bid for the border station was awarded in January 1999. The project was let for approximately \$29 million and the facility became operational on April 15, 2000. The General Services Administration's lease expires April 2012.

Source: General Services Administration, August 2002

Mexico: Plans for the border station were approved by CABIN on December 17, 1997.

Source: SEDICOT, Gobierno del Estado de Tamaulipas, May 1998

BRIDGE CONSTRUCTION STATUS:

U.S.: Groundbreaking occurred on September 30, 1998, and the bridge officially opened on April 15, 2000.

Source: Laredo District, TxDOT, November 2000

Mexico: The bridge officially opened on April 15, 2000.

Source: Laredo District, TxDOT, November 2000

CONNECTING ROADWAY:

U.S.: Loop 20, near FM 1472 and IH-35. Loop 20 connects the International Bridge with FM 1472 and IH-35.

Source: Laredo District, TxDOT, June 2001

Mexico: A 32 km loop connects at Kilometer 22 south of Nuevo Laredo on MEX 85 to Kilometer 12 on MEX 2, northwest of Nuevo Laredo.

Source: SEDICOT, Gobierno del Estado de Tamaulipas, May 1998

IMPROVEMENTS:

U.S.: The interchange construction at the intersection of Loop 20 and IH-35 is ongoing. The project will consist of overpass bridges, reconstruction of frontage roads and two direct connectors.

Source: Laredo District, TxDOT, January 2004

A new direct connector at the intersection of Loop 20 and IH-35, which connects from IH-35 North to Loop 20 West, was completed in April 2003. The project was let in January 2002 at an estimated cost of \$1.1 million.

Source: Laredo District, TxDOT, January 2004

Loop 20 will be widened from a two-lane rural roadway to a five-lane section starting 1.6 miles north of US 59 to McPherson Road. The project was let in August 2001 at an estimated cost of \$14.7 million and construction is ongoing.

Source: Laredo District, TxDOT, January 2004

**Laredo-Colombia Solidarity Bridge
Laredo, Texas – Colombia, Nuevo León**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Colombia Bridge, Puente Solidaridad, Puente Colombia and Puente Internacional Solidaridad Colombia

LOCATION: TxDOT District: Laredo
Texas County: Webb
U.S. City: Laredo
Mexican City: Colombia

DESCRIPTION: An eight-lane bridge and two sidewalks.
Bridge length: 1,216 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-22 240 B01391 001 01

BRIDGE OWNER OR OPERATOR:
U.S.: City of Laredo
Mexican Owner: Government of Mexico. The State of Nuevo León has the concession until 2007.
Source: S.C.T., December 1998
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:
Completed on July 31, 1991

FUNDING/COST: U.S.: The City of Laredo financed the estimated \$12 million cost through revenue bonds on May 21, 1990.
Source: City of Laredo, May 1995

HOURS: 8 a.m. – Midnight (POV – M-Sun)
8 a.m. – Midnight (Commercial/Cargo – M-Fri)
8 a.m. – 4 p.m. (Commercial/Cargo – Sat.)
12 p.m. – 4 p.m. (Commercial/Cargo – Sun.)
Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The City of Laredo and Webb County submitted a Presidential Permit application in 1989, which was approved March 28, 1990.

USCG Bridge permit approved May 8, 1990.

Source: United States Coast Guard

BORDER STATION:

The General Services Administration owns the U.S. border station, which was constructed in 1991.

The U.S. Customs and Border Protection constructed a fixed-site truck x-ray facility at this port in early 1999.

Source: General Services Administration, August 1999

CONNECTING ROADWAY:

U.S.: FM 255, which connects to Camino Colombia (toll road) and FM 1472 (Mines Road).

Source: Laredo District, TxDOT, January 2004

Mexico: MEX 2 (La Ribereña) will be expanded from Kilometer 12 to the Colombia Bridge.

Source: S.C.T., February 1999

IMPROVEMENTS:

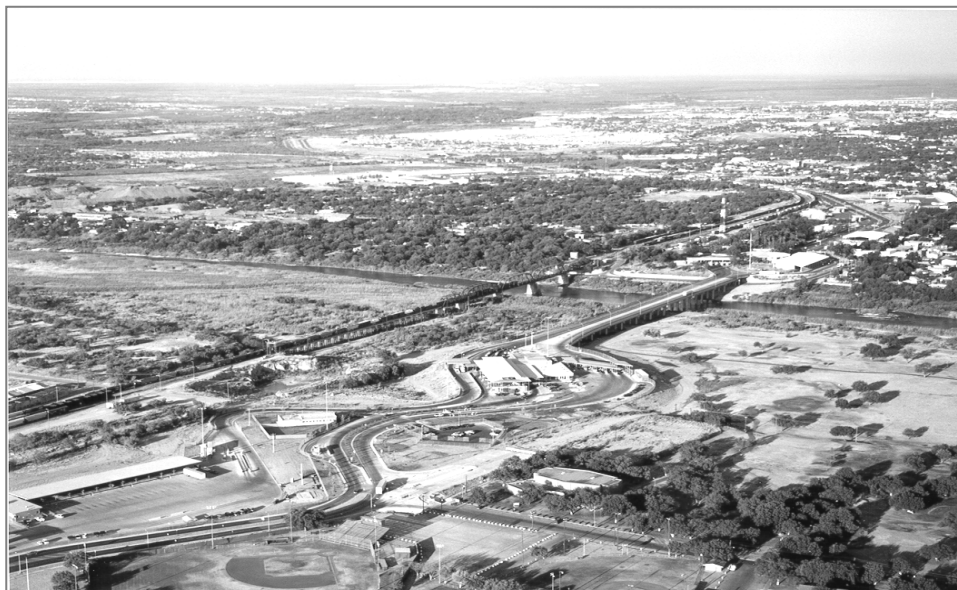
U.S.: The GSA has received TEA-21 funding to expand the truck lanes at the terminal of the bridge to alleviate congestion. The project is on hold awaiting design specifications from the City.

Source: General Services Administration, August 2002

Mexico: Construction of the \$8.36 million four-lane privately owned roadway project was let in May 1990 and opened in August 1991. MEX 2 (La Ribereña) was expanded to four lanes in the corresponding part of Nuevo León from kilometer 5 to 34, this project was completed in early 2002.

Source: Laredo District, TxDOT, January 2004

**Camino Real International Bridge
Eagle Pass, Texas – Piedras Negras, Coahuila**



**The United States is shown at the bottom of the photo.
The Eagle Pass-Piedras Negras Railroad is shown to the left of the
Camino Real International Bridge**

LOCAL NAMES: Eagle Pass-Piedras Negras International Bridge II, Puente Dos, Puente Camino Real and Puente Internacional Coahuila 2000

LOCATION: TxDOT District: Laredo
Texas County: Maverick
U.S. City: Eagle Pass
Mexican City: Piedras Negras

DESCRIPTION: The bridge began operating on September 24, 1999, and is located approximately 0.6 miles south of the existing Eagle Pass International Bridge and immediately north of the international railroad bridge owned and operated by Union Pacific. The bridge structure is 1384 feet in length with 374 feet on the Mexican side and 1,010 feet on the U.S. side. The border station for the Port of Entry facilities consists of approximately 46 acres. The 84-foot wide bridge roadway provides six lanes (three in each direction) and includes two six-foot sidewalks for pedestrians.

Source: City of Eagle Pass, August 2002

BRIDGE OWNER OR OPERATOR:

U.S. Owner: City of Eagle Pass
U.S. Operator: Eagle Pass Bridge System

Source: City of Eagle Pass, May 1998

Mexican Owner: Mexican Government.

Source: Dirección General de Caminos SCOPE, Gobierno de Coahuila, September 1998

Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios
Conexos (CAPUFE)

FUNDING/COST: The City of Eagle Pass provided funding for the \$30 million project.

HOURS: 24 hours (POV)
Hours for Commercial Traffic into U.S.:
7 a.m. – 11 p.m. (M-Fri)
8 a.m. – 4 p.m. (Sat)
Hours for Commercial into Mexico:
9 a.m. –10 p.m. (M-Fri)
11 a.m. –3 p.m. (Sat)
Note: wide loads up to nineteen feet can be accommodated
Source: City of Eagle Pass, March 2004

TOLL: POVs - \$2.00 Buses - \$6.00
Pedestrians - \$0.25 Motorcycles - \$2.00
Commercial Vehicles - \$3.00 plus \$3.00 per axle
Source: City of Eagle Pass, March 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The Department of State issued the Presidential Permit to the City of Eagle Pass in May 1996. The Coast Guard Bridge Permit was issued in August 1996. Final Diplomatic notes authorizing the construction of the bridge were exchanged between the U.S. and Mexican governments in November 1997.

Source: City of Eagle Pass, March 1999

Mexico: The State of Coahuila constructed and operates the bridge and facilities.

Source: S.C.T., November 1999

BORDER STATION:

U.S.: The City of Eagle Pass and GSA have jointly developed a master plan for phased expansion of the border station. The phase I facilities have been in operation for two and a half years. Phase II of the facilities will include six tollbooths with one dedicated for truck traffic, 12 primary inspection stations, 36 secondary inspection stations, four truck inspection stations, an expanded truck dock and expanded support facilities for USDA, Customs and Border Protection. Letting for phase II of the facilities is scheduled for 2006, and will take approximately 24 months to complete.

Source: Laredo District, TxDOT, January 2004

The GSA has received 2002 funding for the design of the permanent border station facilities and received construction funding in 2004.

Source: General Services Administration, March 2004

Mexico: Construction of the border station began in July 1998 and was completed in August of 1999.

Source: SCT, November 1999

CONNECTING ROADWAY:

U.S.: Construction for the Truck Route was divided into two phases. Phase 1A was completed in April 1999 at a cost of \$1.1 million and is approximately 1/2 mile long, connecting at FM 1021-Wilson Street and ending at the Camino Real International Bridge. Phase 1B will be approximately 2 miles, starting at the Camino Real International Bridge and ending at FM 375. Phase 1B will include 2 lanes with shoulders and is re-scheduled for letting in April 2006 at a cost of \$6 million.

Source: Laredo District, TxDOT, January 2004

Mexico: The governors of Coahuila and Zacatecas signed an agreement to expand highway infrastructure from Saltillo to Zacatecas. Highway 57, a four-lane highway from Piedras Negras to Saltillo, Coahuila, is complete with plans for four-lanes all the way to Mexico City.

Source: City of Eagle Pass, March 2004

IMPROVEMENTS:

U.S.: An outer loop from the bridge around the city is scheduled for letting in 2005. Funding has been secured for this project. Phase I construction will extend from the bridge to US Highway 57, with phase II starting at US Highway 57 to US Highway 277 North.

Source: Laredo District, TxDOT, January 2004

Source: City of Eagle Pass, March 2004

Mexico: The State of Coahuila and SCT continue the improvement and construction of the Mexico-Piedras Negras Transport Corridor to the new port of entry. A number of projects are planned in the short to medium term. Included was the construction of two road sections, La Muralla (10.0 km) and Saltillo bypass (36.0 km) at a cost of 327 million pesos, completed in late 2002.

Source: Laredo District, TxDOT, January 2004

Eagle Pass Bridge I
Eagle Pass, Texas – Piedras Negras, Coahuila



The United States is shown at the bottom of the photo.

LOCAL NAMES: Eagle Pass-Piedras Negras International Bridge and Puente Piedras Negras-Eagle Pass

LOCATION: TxDOT District: Laredo
Texas County: Maverick
U.S. City: Eagle Pass
Mexican City: Piedras Negras

DESCRIPTION: A two-lane bridge.
Bridge length: 1,855 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-22 159 B00290 001 01

BRIDGE OWNER OR OPERATOR:
U.S.: City of Eagle Pass
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:
Originally constructed in 1927 and reconstructed in 1954, the bridge was rehabilitated in 1985.
Source: General Services Administration

FUNDING/COST: The City of Eagle Pass purchased the bridge on March 17, 1947, from Francisco Estrada for \$320,000.

Source: Laredo District, TxDOT

HOURS: 7 a.m. - 11 p.m. (POV only – M-Sun)

Source: U.S. Customs and Border Protection, January 2004

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits were not required for bridges built before 1972.

BORDER STATION:

The U.S. border station, owned by the General Services Administration, was completed in 1960 and expanded in 1991. Commercial traffic was moved to the Camino Real International Bridge when it opened in 1999. Reuse of the commercial facilities will be determined by Customs and Border Protection in the near future.

Source: General Services Administration, March 2004

CONNECTING ROADWAY:

U.S.: US 57 connects to US 277

Mexico: Near MEX 2 and MEX 57

IMPROVEMENTS:

U.S.: US 57 was expanded from two lanes to four lanes with shoulders for approximately 14 miles outside the city limits.

Source: City of Eagle Pass, August 2002

A US 277 (Business) reconstruction and widening project let for \$3.8 million was completed in May 1999. A reconstruction on US 277 (Business) consisting of the roadway from Ceylon St. to US 57 let in May 2001 at an estimated cost of \$2.23 million. The project was completed in August 2003.

Source: Laredo District, TxDOT, January 2004

Source: City of Eagle Pass, March 2004

**Del Rio-Ciudad Acuña International Bridge
Del Rio, Texas – Ciudad Acuña, Coahuila**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Del Rio International Bridge, Puente Acuña, and Puente Ciudad Acuña-Ciudad Del Rio

LOCATION: TxDOT District: Laredo
Texas County: Val Verde
U.S. City: Del Rio
Mexican City: Ciudad Acuña

DESCRIPTION: A four-lane bridge.
Bridge length: 2,035 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-22 233 B00770 002 01

BRIDGE OWNER OR OPERATOR:
U.S.: City of Del Rio
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios
Conexos (CAPUFE)

YEAR OF CONSTRUCTION:
Originally constructed in 1930 and reconstructed in 1987.
Source: City of Del Rio, May 1995

HOURS: 24 hours (POV)
Only one POV lane is open between 12:30 a.m. – 8:30 a.m.
Source: City of Del Rio, February 2004

8 a.m. – 9 p.m. (Commercial/Cargo – M-Fri)
9 a.m. – 5 p.m. (Commercial/Cargo – Sat.)
Source: U.S. Customs and Border Protection, January 2004

TOLL: POVs - \$2.00 Buses - \$4.50
Pedestrians - \$0.25 RVs - \$4.00
Bicycles - \$0.25
Source: City of Del Rio, August 2002

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The City of Del Rio submitted a Presidential Permit application, which was approved in 1986.

BORDER STATION:

U.S.: The U.S. border station was constructed in 1967, and the City of Del Rio's portion was reconstructed in 1995. The General Services Administration owns the border station and prepared a master plan for a major three-phase expansion of the border station. The first two phases, which included a new import lot, are complete. Funding for the third phase was received in 2002 and design is underway. Construction funding has been approved for 2004.

Source: General Services Administration, March 2004

Mexico: The border station was to be completed in three phases. The first two phases are complete, while the third phase has not yet been clearly defined.

Source: SCT, January 1999

CONNECTING ROADWAY:

U.S.: US 277/Spur 239 connecting with US 90

Mexico: Near MEX 2, Coahuila State Highway 29 connecting with MEX 57

IMPROVEMENTS:

U.S.: A US 90 reconstruction project from Avenue F to Bedell Ave., and from the Laughlin AFB to 1.5 miles east of Bedell Ave., was relet in September 1999 at an estimated cost of \$4.5 million. The project was completed on March 7, 2002.

Source: Laredo District, TxDOT, August 2002

**Lake Amistad Dam Crossing
Del Rio, Texas – Ciudad Acuña, Coahuila**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Amistad Dam, Presa la Amistad

LOCATION: TxDOT District: Laredo
Texas County: Val Verde
U.S. City: Del Rio
Mexican City: Ciudad Acuña

DESCRIPTION: A two-lane road runs above the dam.
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, February 1997

DAM OWNER OR OPERATOR:
U.S: U.S. Section, International Boundary and Water Commission
Mexico: Mexican Section, International Boundary and Water Commission
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, February 1997

PORT-OF-ENTRY OWNERS AND OPERATORS:
U.S: Immigration and Naturalization Service

Mexico: Mexican Customs
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, February 1997

YEAR OF CONSTRUCTION:
1969
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

HOURS: 10 a.m. – 6 p.m. (POV only)
Source: U.S. Customs and Border Protection, January 2004

TOLL: No

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for structures built before 1972.

BORDER STATION:

The U.S. border station, owned by the Immigration and Naturalization Service, was completed in 1969.

Source: General Services Administration, Summary of Existing and Proposed Border Stations, May 1995

CONNECTING ROADWAY:

U.S.: Spur 349 connects to US 90

Mexico: Near MEX 2

**Presidio Bridge
Presidio, Texas - Ojinaga, Chihuahua**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Presidio-Ojinaga Bridge, Puente Ojinaga

LOCATION: TxDOT District: El Paso
Texas County: Presidio
U.S. City: Presidio
Mexican City: Ojinaga

DESCRIPTION: A two-lane bridge
This bridge replaced a privately owned, antiquated wooden structure located about 500 yards down river.

Source: General Services Administration

Bridge length: 791 feet

Source: Bridge Inventory and Inspection File, TxDOT

Bridge identification number-24 189 010410 053 01

BRIDGE OWNER OR OPERATOR:

U.S. Owner: State of Texas

U.S. Operator: Texas Department of Transportation

Mexican Owner: Government of Mexico

Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:

1985

Source: Bridge Inventory and Inspection File, TxDOT

FUNDING/COST: **U.S.:** \$869,113 for bridge and approach. Mexico paid for half of this seven span bridge. Since there were an odd number of spans, Mexico paid for the superstructure of the middle span and the U.S. paid for the substructure.

Source: Design Division, TxDOT

HOURS: 24 hours (POV)
9 a.m. – 5 p.m. (Commercial/Cargo)
Source: U.S. Customs and Border Protection, January 2004

TOLL: No toll southbound
Northbound toll - \$1.80 (POV)
\$3.80 (Commercial Trucks)
Source: U.S. Customs and Border Protection, January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidio County submitted a Presidential Permit application, which was approved on July 2, 1976. The permit was amended and transferred to the State of Texas on May 4, 1982.

Source: U.S. State Department

BORDER STATION:

U.S.: The U.S. border station was completed in 1987. A private individual (Richard Slack) owns the U.S. border station, which is leased to the General Services Administration (GSA). GSA's lease, which was to expire in 2007, was renegotiated and provided for the expansion of the facilities to include additional office and dock areas, a primary inspection area and a permanent kennel. GSA's lease expires in July 2022.

Source: General Services Administration, March 2004

Mexico: The Government of Mexico, Customs and Immigration operate the border station.

Source: Centro S.C.T. Chihuahua, June 2001

CONNECTING ROADWAY:

U.S.: US 67 and FM 170

Mexico: MEX 16 and CHIH 18

Source: Centro S.C.T. Chihuahua

IMPROVEMENTS:

Mexico: The new state highway Coyame – Potrero del Llano (La Mula) connecting MEX 16 and CHIH 18 is complete and became operational in September 2003. The new route starts off of Highway 16 about 25 miles south of Coyame (near the village-El Huerfano) and runs parallel to the Chihuahua al Pacifico railroad and connects to the Camargo Highway (CHIH 18) near La Mula (Potrero del Llano). With this 70-mile bypass, travelers are able to avoid the sharp curves and steep grades of Highway 16 through Peguis Canyon.

Source: TxDOT, January 2004

Source: Centro S.C.T. Chihuahua, January 2004

**Fort Hancock-El Porvenir Bridge
Fort Hancock, Texas – El Porvenir, Chihuahua**



The United States is shown at the bottom of the photo.

LOCAL NAME: Puente El Porvenir

LOCATION: TxDOT District: El Paso
Texas County: Hudspeth
U.S. City: Fort Hancock
Mexican City: El Porvenir

DESCRIPTION: A narrow two-lane bridge.
Bridge length: 510 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-24 116 AA8883 001 01

BRIDGE OWNER OR OPERATOR:

U.S. Owner: U.S. Section, International Boundary and Water Commission

U.S. Operator: U.S. Customs

Mexican Owner: Government of Mexico

Mexican Operator: Government of Mexico and CILA

Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

Source: Centro S.C.T. Chihuahua, June 2001

YEAR OF CONSTRUCTION:

Built in 1936.

Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

This is one of three small, light-duty bridges built by the International Boundary and Water Commission as part of U.S.-Mexico, Rio Grande Rectification Project.

Source: International Boundary and Water Commission, U.S. Section, November 2000

FUNDING/COST: Built using U.S. and Mexican federal funds.

HOURS: 6 a.m. – 10 p.m. (POV/Light Truck)

Source: U.S. Customs and Border Protection, January 2004

TOLL: None

Source: U.S. Customs and Border Protection, January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for bridges built before 1972.

BORDER STATION:

U.S.: The current border station is a small facility that was constructed in 1955 and is owned by the Immigration and Naturalization Service. The design for a new border station at this location was completed in May 2002. GSA received construction funding for the project in February 2002, and construction was completed in April 2003.

Source: General Services Administration, March 2004

Mexico: The Mexican Government operates the border station.

Source: Centro S.C.T. Chihuahua, June 2001

CONNECTING ROADWAY:

U.S.: FM 1088 connects with Texas 20

Mexico: Connects to MEX 2

**Fabens-Caseta Bridge
Fabens, Texas – Caseta, Chihuahua**



The United States is shown at the bottom of the photo.

LOCAL NAME: Puente La Caseta and Tornillo-Guadalupe

LOCATION: TxDOT District: El Paso
Texas County: El Paso
U.S. City: Fabens
Mexican City: Caseta

DESCRIPTION: A narrow two-lane bridge.
Bridge length: 510 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-24 072 AA8884 001 01

BRIDGE OWNER OR OPERATOR:

U.S. Owner: U.S. Section, International Boundary and Water Commission
U.S. Operator: U.S. Customs
Mexican Owner: Government of Mexico
Mexican Operator: Mexican Customs
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

YEAR OF CONSTRUCTION:

Built in 1938.
Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

This is one of three small, light-duty bridges built by the International Boundary and Water Commission as part of U.S.-Mexico, Rio Grande Rectification Project. The Fabens-Caseta Bridge is to be demolished once the new Guadalupe-Tornillo International Bridge is built to replace it.

Source: International Boundary and Water Commission, U.S. Section, November 2000

FUNDING/COST: Built using U.S. and Mexican federal funds.

HOURS: 6 a.m. - 10 p.m. (POV/Light Truck)

Source: U.S. Customs and Border Protection, August 2002

TOLL: None

Source: U.S. Customs and Border Protection, June 2001

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for bridges built before 1972.

BORDER STATION:

A United States temporary border station was constructed on 7.59 acres of land northwest of the existing Port of Entry. This facility includes additional inspection lanes, parking and modular office trailer units.

Source: General Services Administration, August 2002

CONNECTING ROADWAY:

U.S.: FM 1109 connects with Texas 20, FM 76 and FM 793, which connect to I-10. Island Guadalupe and Tornillo-Guadalupe roads run concurrently with FM 1109.

Mexico: A state road connects with MEX 2

IMPROVEMENTS:

U.S.: The Texas Transportation Commission approved the state bridge permit on March 27, 2003 for the new Guadalupe/Tornillo (Fabens) Bridge, to be located upstream from the current Fabens-Caseta Bridge. The County of El Paso's Presidential Permit application is pending approval by the Department of State. Pending the approval of the Permit, the County is proceeding with the first phase of roadway construction plans from the new Port to SH 20. The plans are complete and funding is available.

Source: TxDOT, January 2004

**Ysleta-Zaragoza Bridge
Ysleta, Texas – Zaragoza, Chihuahua**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Zaragoza Bridge, Puente Zaragoza and Puente Ysleta-Zaragoza

LOCATION: TxDOT District: El Paso
Texas County: El Paso
U.S. City: El Paso
Mexican City: Ciudad Juárez

DESCRIPTION: There are two structures, one is a four-lane bridge for commercial traffic, and the other is a four-lane bridge for non-commercial traffic. The non-commercial bridge also has two pedestrian walkways. The walkways join an elevated pedestrian bridge that connects to the second floor of the INS Administration Building.

Source: El Paso District, TxDOT, December 1998

Bridge length: 804 feet northbound and southbound

Source: Bridge Inventory and Inspection File, TxDOT

Bridge identification numbers - 24 072 B74640 001 01 northbound truck bridge, 24 072 B74640 002 01 southbound vehicular bridge

BRIDGE OWNER OR OPERATOR:

U.S.: City of El Paso

Mexican Owner: Government of Mexico. Promofront S. A. de C. V. has the concession until 2017.

Source: Secretaría de Relaciones Exteriores, Puertos y Servicios Fronterizos, January 1997

Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:

The original bridge was constructed in 1938 as part of the U.S.-Mexico River rectification project.

Source: International Boundary and Water Commission, U.S. Section, July 1994

Rebuilt in 1955 and rebuilt again in 1990.

Source: El Paso District, TxDOT

FUNDING/COST: U.S.: \$3.54 million

Source: El Paso District

HOURS:

24 hours (POV)

8 a.m. – Midnight (Commercial/Cargo – M-F)

Source: U.S. Customs and Border Protection, January 2004

TOLL:

Southbound: \$1.65 (POV)

\$3.00 per axle (Commercial)

Northbound: \$1.65 (POV)

\$4.30 2-axle (Commercial)

\$9.00 5-axle (Commercial)

\$14.30 6-axle (Commercial)

Source: U.S. Customs and Border Protection, January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The City of El Paso submitted a Presidential Permit application for the replacement bridges, which was approved on June 30, 1987.

Source: U.S. State Department

BORDER STATION:

U.S.: The border station was completed in August 1992. The General Services Administration owns the U.S. border station and 61 acres of the border station's land area. The city operates truck tollbooths on the GSA property. The GSA expanded the facility in 1998 by adding two-bay empty truck inspection, a HAZMAT containment area, a narcotic storage building, additional truck staging, and miscellaneous site improvements. A fixed-site truck x-ray system that enables them to x-ray an entire truck in about 10 minutes has been in place since January 1998.

Source: General Services Administration, August 1999

Source: U.S. Customs and Border Protection, U.S. Department of Treasury, June 1998

North American Trade Automation Prototype dedicated short-range testing equipment was installed in July 1997. For more information on the NATAP program on the Internet, see <http://www.itds.treas.gov>

Source: Federal Highway Administration, Texas Division, August 1997

The region has submitted a prospectus for a FY 05 design. The project proposes an increase in the pedestrian and vehicle primary areas, the import lot area, and additional site security and parking.

Source: General Services Administration, March 2004

CONNECTING ROADWAY:

U.S.: Near State Loop 375, Border Highway, and Americas Avenue. Connects to I-10.

Mexico: A state road connects with MEX 2 and continues to MEX 45.

IMPROVEMENTS:

U.S. The \$25 million dollar project, which was let in March 1999 and added four main lanes to the frontage roads of Loop 375 from the Zaragoza Port of Entry to IH 10, is complete.

Source: El Paso District, TxDOT, February 2004

The city of El Paso's update to the existing toll collection system should be completed by the end of February 2004.

Source: Toll Collection Superintendent, February 2004

A consultant firm conducted a feasibility study for the extension of the Border Highway from the Zaragoza Port of Entry to the Fabens Port of Entry. The \$1 million study began during the summer of 1995 and was completed in February 1997. The study recommended the extension, which is on the long-range plan.

Source: El Paso District, TxDOT, February 2004

A project to open a Dedicated Commuter Lane (DCL) by converting the existing bridge from 4 to 5 lanes and adding a third booth on the Mexican side is scheduled for FY 2004.

Source: El Paso District, TxDOT, January 2004

Mexico: The access roads to the bridge are both state and city owned. The City of Juárez is in the process of modernizing the system of city roads that lead up to the bridge. Boulevard Francisco Villarreal joins MEX 45 with MEX 2.

Source: Centro S.C.T. Chihuahua, June 2001

Bridge of the Americas
El Paso, Texas – Ciudad Juárez, Chihuahua



The United States is shown at the top of the photo.

LOCAL NAMES: Puente Rio Bravo, Puente Internacional Cordova-Las Americas, Cordova Bridge, Puente Libre, BOTA and Free Bridge

LOCATION: TxDOT District: El Paso
Texas County: El Paso
U.S. City: El Paso
Mexican City: Ciudad Juárez

DESCRIPTION: Four separate structures, 2 two-lane bridges for truck traffic, and 2 four-lane bridges for other vehicular traffic.

Bridge length: 506 feet

Source: Bridge Inventory and Inspection File, TxDOT

Bridge identification numbers-24 072 255204 053 02 northbound truck bridge, 24 072 255204 054 02 northbound auto bridge, 24 072 2555204 055 02 southbound auto bridge, 24 072 255204 056 02 southbound truck bridge

Source: El Paso District, TxDOT, December 1998

U.S.: Construction of four replacement bridges began in July 1996, and was completed in July 1998. These four bridges included two bridges for northbound and southbound commercial traffic, and two bridges for northbound and southbound passenger vehicles. The bridges provide a total of eight lanes for passenger vehicles, four lanes for trucks and two sidewalks for pedestrians.

Source: Office of Bridge Administration, U. S. Coast Guard, December 1998

Source: El Paso District, TxDOT, December 1998

Mexico: In September 1993, by means of IBWC Minute 290, both governments agreed to replace the current bridge. The decision was based on the high risks involved with the structural condition of the former bridge.

Source: International Boundary and Water Commission, U.S. Section, January 1997

BRIDGE OWNER OR OPERATOR:

U.S. Owner: U.S. Section, International Boundary and Water Commission

U.S. Operator: U.S. Customs

Mexican Owner: Mexican Section, International Boundary and Water Commission

Mexican Operator: Mexican Customs

Source: International Boundary and Water Commission, U.S. Section, July 1994

YEAR OF CONSTRUCTION:

Original: 1967 (as part of the Chamizal Treaty work, U.S. and Mexico)

The replacement bridges were completed in June 1998.

Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

FUNDING/COST: U.S.: Cost was \$6 million, with a similar amount financed by fee collections from commercial traffic, according to an agreement between commercial interests and Mexico.

Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

HOURS:

24 hours (POV)

6 a.m. – 6 p.m. (Commercial/Cargo – M-F)

6 a.m. – 2 p.m. (Commercial/Cargo – Sat.)

Source: U.S. Customs and Border Protection, January 2004

TOLL:

Passenger vehicles and pedestrians cross toll free. Mexico does not collect fees from northbound or southbound commercial vehicles.

Source: U.S. Customs and Border Protection, June 2001

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Replacement according to Coast Guard Bridge Permit dated September 28, 1995.

Source: International Boundary and Water Commission, U.S. Section, El Paso, TX, November 1996

BORDER STATION:

U.S.: The U.S. border station, completed in 1967, is owned by the General Services Administration and was renovated in 1992 along with the U.S. connecting roadway. An expansion of the import lot was completed in 1998.

Source: General Services Administration, August 1999

CONNECTING ROADWAY:

U.S.: I-110 to US 62 and US 54, which connects to I-10

Mexico: Near MEX 45

IMPROVEMENTS:

\$3.9 million was approved through the TEA-21 program for relocation of the Federal Inspection Services (FIS), headhouse and construction of four additional non-commercial inspection lanes. The contract for construction was signed on September 18, 2001, with substantial completion in September 2003. The total cost for the projects exceeded \$4.8 million with the remaining funds provided by GSA, Customs, INS and USDA.

Source: General Services Administration, March 2004

Source: U.S. Customs and Border Protection, August 2002

Good Neighbor Bridge
El Paso, Texas – Ciudad Juárez, Chihuahua



The United States is shown at the top of the photo.
The Paso del Norte Bridge is shown on the left. The Good Neighbor Bridge is shown on the right.

LOCAL NAMES: Stanton Street Bridge, Friendship Bridge, Puente Rio Bravo and Puente Ciudad Juárez-Stanton El Paso
Source: El Paso District, TxDOT, August 1999

LOCATION: TxDOT District: El Paso
Texas County: El Paso
U.S. City: El Paso
Mexican City: Ciudad Juárez

DESCRIPTION: Four-lane southbound vehicular bridge. No commercial traffic allowed.
Bridge Length: 880 feet
Source: El Paso District, TxDOT, December 1998
Bridge identification number-24 072 255204 023 02
Source: El Paso District, TxDOT, December 1998

BRIDGE OWNER OR OPERATOR:
U.S.: City of El Paso
Mexican Owner: Government of Mexico

YEAR OF CONSTRUCTION:
Rebuilt in 1967 as part of the Chamizal Treaty.
Source: International Boundary and Water Commission, U.S. Section

FUNDING/COST: Built from U.S. and Mexican federal funds.

HOURS: 6 a.m. – 10 p.m. Mon-Fri. (DCL Northbound – POV only)
10 a.m. – 6 p.m. Sat. and Sun. (DCL Northbound – POV only)
24 hours (Southbound – POV Only)
Source: U.S. Customs and Border Protection, January 2004
Source: Toll Collection Superintendent, February 2004

TOLL: \$1.65
Source: U.S. Customs and Border Protection, January 2004
Source: Toll Collection Superintendent, February 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for bridges built before 1972.

BORDER STATION:

The U.S. border station, owned by the General Services Administration, was completed in 1967. The GSA leases the facilities to the City of El Paso. Pedestrians and inbound traffic are not permitted on this bridge.
Source: General Services Administration, August 2002

A northbound Dedicated Commuter Lane (DCL), which utilizes the SENTRI system, became operational on September 7, 1999. One lane of the bridge was converted for northbound traffic to allow for the DCL into El Paso. A DCL processing facility was constructed adjacent to the bridge to the west and is staffed by the Federal Inspection Services.
Source: U.S. Customs and Border Protection, June 2001

CONNECTING ROADWAY:

U.S.: US 62
Stanton Street runs concurrently with US 62.

Mexico: Near MEX 2, MEX 45

IMPROVEMENTS:

U.S.: Rehabilitation of the existing bridge and updating of the toll collection systems at a cost of approximately \$8.9 million is underway. The city of El Paso acquired a SIB loan for the rehabilitation and work began on FY 2003. The rehabilitation is 90% complete and scheduled to be completed by the end of March 2004. The toll system upgrade is scheduled to be completed by the end of February 2004.
Source: El Paso District, TxDOT, January 2004
Source: Toll Collection Superintendent, February 2004

**Paso Del Norte Bridge
El Paso, Texas – Ciudad Juárez, Chihuahua**



The United States is shown at the bottom of the photo.

LOCAL NAMES: Paso del Norte Bridge, Santa Fe Street Bridge, Puente Benito Juárez, Puente Paso del Norte and Puente Juárez-Santa Fe

LOCATION: TxDOT District: El Paso
U.S. City: El Paso
Mexican City: Ciudad Juárez

DESCRIPTION: A four-lane bridge used for northbound, non-commercial traffic only.
Bridge length: 982 feet
Source: Bridge Inventory and Inspection File, TxDOT
Bridge identification number-24 072 255204 022 02
Source: El Paso District, TxDOT, December 1998

BRIDGE OWNER OR OPERATOR:
U.S. Owner: City of El Paso
Mexican Owner: Government of Mexico
Mexican Operator: Caminos y Puentes Federales de Ingresos y Servicios Conexos (CAPUFE)

YEAR OF CONSTRUCTION:
Rebuilt in 1967 as part of the Chamizal Treaty
Source: International Boundary and Water Commission, U. S. Section

FUNDING/COST: U.S. and Mexican federal funds.

HOURS: 24 hours (POV only)
Source: U.S. Customs and Border Protection, January 2004

TOLL: \$1.65 (Northbound only)
Source: U.S. Customs and Border Protection, January 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for bridges built before 1972.

BORDER STATION:

U.S.: The U.S. border station was completed in 1967 as a result of the Chamizal Treaty when the channel of the Rio Grande River was moved. The General Services Administration owns the U.S. border station - which was renovated in 1991.

Source: Border Station Task Force Book, November 1996

A prospectus project to design the expansion and renovation is underway. The project will include adding to and renovating the administration building, a new kennel, renovating the existing kennel into administration space, relocating the headhouse, expanding the number of primary inspection lanes with the addition of two new primary booths, and reconfiguring the secondary inspection area and adding super booths. The plans also include the demolition of the TABC building and additional surface parking. Because of the significant phasing required for this project, to keep as many lanes open as possible, the estimated construction period is estimated at 24 to 36 months.

Source: General Services Administration, March 2004

CONNECTING ROADWAY:

U.S.: US 85, El Paso Street runs concurrently with US 85

Mexico: Near MEX 2

IMPROVEMENTS:

U.S.: The city of El Paso acquired a SIB loan for the rehabilitation of the existing bridge and updating the toll collection system. The rehabilitation is 90% complete and scheduled to be completed by the end of March 2004. The toll system upgrade is scheduled to be completed by the end of February 2004.

Source: El Paso District, TxDOT, January 2004

Source: Toll Collection Superintendent, February 2004

Appendix I

Proposed Bridges

**Port of Brownsville Bridge
Brownsville, Texas – Matamoros, Tamaulipas**

LOCAL NAMES: Port of Brownsville-Matamoros Bridge and Puente Internacional del Puerto de Brownsville

LOCATION: TxDOT District: Pharr
Texas County: Cameron
U.S. City: Brownsville
Mexican City: Matamoros

DESCRIPTION: A four-lane vehicular bridge. A single-track railroad bridge is to be developed at a later date. The sponsor has requested that the crossing be used exclusively for commercial traffic.
Source: U.S. Department of State, December 1996

BRIDGE OWNER OR OPERATOR:
U.S.: Port of Brownsville

FUNDING/COST: U.S.: The \$31 million for bridge, federal facilities and connecting roads is to be funded completely by the Port of Brownsville through General Obligation Bonds, if project financing does not become available.
Source: Pharr District, TxDOT, March 1999
Source: Raul Besteiro, CEO/Port Director, Port of Brownsville, November 1999

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:
U.S.: The Brownsville Navigation District (BND)/Port of Brownsville submitted an amended Presidential Permit application and environmental assessment on August 14, 1995. The U.S. Department of State has received all information requested from the Port of Brownsville and on October 9, 1997, the Department of State made a finding that two international bridges sponsored by the Brownsville Navigation District (BND) would have no significant impact on the environment. The BND's Presidential Permit was signed on October 12, 1997, and issued on November 3, 1997. The Port has obtained a Section 404 Wetlands Permit and a Section 401 Water Quality Certification from the Texas Natural Resource Conservation Commission (TNRCC).
Source: Pharr District, TxDOT, March 1999
Source: Raul Besteiro, CEO/Port Director, Port of Brownsville, November 1999

No Coast Guard permit application received yet.
Source: Office of Bridge Administration, U.S. Coast Guard, June 2001

CONNECTING ROADWAY:

U.S.: The Port will construct a new road to connect South Port Road to the bridge and will extend the port railroad to the bridge site crossing State Highway (SH) 4. The connecting roadway will be funded 100 percent by the Port.

Source: Pharr District, TxDOT, March 1999

BRIDGE CONSTRUCTION STATUS:

U.S.: A consultant for the Port Authority is working on the design of the future facilities. Coordination with Mexico is ongoing.

Source: Pharr District, TxDOT, February 2004

**Donna International Bridge
Donna, Texas - Rio Bravo, Tamaulipas**

LOCAL NAMES: Donna/Rio Bravo Bridge, Puente Rio Bravo-Donna

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Donna
Mexican City: Rio Bravo

DESCRIPTION: An eight-lane bridge is proposed.
Source: Pharr District, TxDOT
Bridge will be approximately 2700 feet long.
Source: City of Donna, December 1996

BRIDGE OWNER OR OPERATOR:
U.S.: City of Donna

FUNDING/COST: U.S.: \$15 million for bridge, land acquisition, and access roads. In October 2001, the Donna-Mercedes Bridge Corporation was formed to construct and operate the bridge.
Source: City of Donna, January 2004

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:
U.S.: The City of Donna received a Presidential Permit on August 22, 1979. Feasibility, environmental and preliminary engineering studies have been completed and approved by federal and state agencies.
Source: City of Donna, January 2004

Mexico: The Mexican sponsor for the bridge project is Grupo COIN who has completed the necessary engineering and environmental studies. Grupo COIN has received approval from the necessary federal agencies to construct the bridge.
Source: City of Donna, January 2004

BRIDGE CONSTRUCTION STATUS:
U.S.: Pending
Mexico: Pending, subject to permit.
Source: City of Donna, January 2004

CONNECTING ROADWAY:
U.S.: FM 493

Mexico: Highway 112
Source: Centro S.C.T. Chihuahua, June 2001

**Anzalduas International Bridge
Hidalgo, Texas –Reynosa, Tamaulipas**

LOCAL NAMES: Sharyland Bridge, Anzalduas International Crossing and Puente Internacional Reynosa-McAllen

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Hidalgo
Mexican City: Reynosa

DESCRIPTION: A four-lane vehicular undivided bridge would be constructed initially with sufficient right-of-way for expansion to an eight-lane divided bridge. This facility is a candidate for the Model Port of Entry whose design criteria includes the latest in intelligent transportation systems and weighing technology.

Source: *Anzalduas International Crossings, Road and Rail*, Halff Associates, Inc. Prepared for the McAllen, Hidalgo, Mission International Bridge Board, December 1993.

Source: Pharr District, TxDOT, February 2004

BRIDGE OWNER OR OPERATOR:

U.S.: Cities of Hidalgo, McAllen and Mission

FUNDING/COST: **U.S.:** \$2.8 million for city toll booths, \$12.9 million for bridge and international road, \$5.8 million for access road

Source: *Anzalduas International Crossings*, Martin Molloy, Halff Associates, Inc. February 1995

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The cities of Hidalgo, Mission and McAllen submitted a revised application for a Presidential Permit dated March 1996 to the State Department for review. In September 1998, the sponsors provided the “1998 Anzalduas Bridge Traffic Study” and supplemental information related to the Presidential Permit to the Department of State for review. The Presidential Permit, which was issued in July 1999, includes caveats outlining minimum traffic volumes at the Pharr/Reynosa Bridge with specific timelines before the bridge can be let to contract.

Source: *Presidential Permit, Number 99-01*, Department of State, July 1999

Source: Pharr District, TxDOT, August 1999

In May 2003, the Anzalduas Bridge Board requested amendments to the Presidential Permit to change the hours of operation from 12 to 24, and to allow for cargo traffic. The amendment requests are under consideration by the Department of State.

Source: TxDOT, International Relations Office, January 2004

Mexico: In the process of being approved by CABIN.

Source: Centro S.C.T. Chihuahua, June 2001

BRIDGE CONSTRUCTION STATUS:

Construction is pending. Per the permit caveats, construction of the bridge was not to begin sooner than April 1, 2003, and the bridge is not to open sooner than January 1, 2005.

Source: *Presidential Permit, Number 99-01*, Department of State, July 1999

Source: TxDOT, International Relations Office, January 2004

CONNECTING ROADWAY:

U.S.: TxDOT is committed to a connection from FM 1016 to the proposed bridge. Estimated cost is \$1.0 million.

Source: Pharr District, TxDOT, August 2002

IMPROVEMENTS:

U.S.: FM 1016, from US 83 to Bentsen Road. Roadway will be widened to four lanes with shoulders. Phase I (Bentsen to Madero) was let in August 2000 and was completed in 2002. Phase II of the Madero to US 83 project, (FM 1016 from US 83 to Madero) was let in March 2003 for \$3.6 million and is under construction.

Source: Pharr District, TxDOT, February 2004

Mission International Bridge
Mission, Texas – Reynosa, Tamaulipas

LOCAL NAMES: Mission Bridge, Puente Internacional Reynosa-Mission, and Mission-Madero Bridge

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Mission
Mexican City: Reynosa

DESCRIPTION: A four-lane vehicular and railroad bridge is proposed.
Source: Design Division, TxDOT

BRIDGE OWNER OR OPERATOR:
U.S.: City of Mission

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:
U.S.: The City of Mission submitted a Presidential Permit application, which was approved December 29, 1978. A USCG bridge permit issued in 1980 became null and void because construction was not commenced in a timely manner. The City of Mission will need to apply for another permit before beginning construction.
Source: City of Mission
Source: Office of Bridge Administration, U.S. Coast Guard, November 2000

BRIDGE CONSTRUCTION STATUS:
Pending

CONNECTING ROADWAY:
U.S.: New road to interconnect FM 1016

Los Ebanos International Bridge
Los Ebanos, Texas – Gustavo Díaz Ordaz, Tamaulipas

LOCAL NAMES: Los Ebanos Bridge, Puente Internacional Díaz Ordaz-Los Ebanos

LOCATION: TxDOT District: Pharr
Texas County: Hidalgo
U.S. City: Los Ebanos
Mexican City: Gustavo Díaz Ordaz

DESCRIPTION: A four-lane bridge is proposed to replace the Los Ebanos Ferry.
Source: Design Division, TxDOT

BRIDGE OWNER OR OPERATOR:
U.S.: Reyna Family

FUNDING/COST: **U.S.:** The estimated cost would be \$5 million for the structure. Total project cost is \$6.5 million.

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permit pending. Updating environmental assessment.
Source: Randolph DeLay, Consultant

Mexico: A committee determined that the permit for the existing crossing could be used.
Source: Centro S.C.T. Chihuahua, June 2001

The environmental assessment is complete.
Source: Presentation at the U.S.-Mexico Binational Bridges and Border Crossings Group meeting, October 2002

BRIDGE CONSTRUCTION STATUS:
Pending

CONNECTING ROADWAY:
U.S.: FM 886 to US 83
Mexico: Near MEX 2

Laredo V International Bridge
Laredo, Texas – Nuevo Laredo, Tamaulipas

LOCAL NAMES: Puente Internacional Laredo IV

LOCATION: TxDOT District: Laredo
Texas County: Webb
U.S. City: Laredo
Mexican City: Nuevo Laredo

DESCRIPTION: Not available.

BRIDGE OWNER OR OPERATOR:
U.S.: Not Available
Mexico: Not Available

FUNDING/COST: **U.S.:** Not available
Mexico: Not available

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:
U.S.: Pending
Mexico: Approvals pending

BRIDGE CONSTRUCTION STATUS:
Pending permit approvals.

CONNECTING ROADWAY:
U.S.: Not Available
Mexico: Not Available

**Guadalupe-Tornillo International Bridge
Fabens, Texas – Caseta, Chihuahua**

LOCAL NAMES: Puente Internacional Tornillo-Guadalupe

LOCATION: TxDOT District: El Paso
Texas County: El Paso
U.S. City: Fabens
Mexican City: Caseta

DESCRIPTION: A six-lane bridge is proposed to replace the Fabens-Caseta Bridge. The bridge would feature three travel lanes in each direction, with one lane designated for commercial traffic and the other two for passenger vehicles. Two pedestrian walkways would also be provided. The county has designated 272 acres of land for the new border facilities and for future expansions. Twenty-two acres will be reserved for the TxDOT-DPS-USDOT truck safety inspection station.

Source: El Paso District, TxDOT, September 2002

BRIDGE OWNER OR OPERATOR:

U.S.: El Paso County

Mexico: State of Chihuahua

FUNDING/COST: **U.S.:** The construction cost is approximately \$11.7 million for the structure. Total project cost is approximately \$16.3 million.

Source: El Paso County, September 2002

TOLL: Yes

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: The Texas state bridge permit for the new Guadalupe/Tornillo Bridge, to be located upstream from the current Fabens-Caseta Bridge was approved by the Texas Transportation Commission in March 2003. The County of El Paso submitted the Presidential Permit application to the Department of State for review/approval on April 14, 2003. The Presidential Permit approval is pending.

Source: TxDOT, International Relations Office, January 2004

Mexico: The State of Chihuahua has completed the draft Environmental Assessment, draft Financial Feasibility Study and the draft Economic Development Plan.

Source: El Paso District, TxDOT, September 2002

BRIDGE CONSTRUCTION STATUS:

Pending permit approvals.

CONNECTING ROADWAY:

U.S.: From IH 10 the primary access route utilizes FM 973 through Fabens, FM 76 south of Fabens and then FM 1109 to the international bridge.

The County is prepared to begin construction of the new location roadway. The first phase of roadway construction will be from the new Port to SH 20. The letting for the project is pending the approval of the Permit. The plans are complete and funding is available.

Source: TxDOT, El Paso District, February 2004

Mexico: The executive design for highway Dr. Porfirio Parra-Samalayuca, which will join Mx 2 and Mx 45, is complete. Construction of this highway is due to start as soon as the new bridge construction begins.

Source: TxDOT, El Paso District, February 2004

Appendix II

Inactive Bridges

Roma International Suspension Bridge
Roma, Texas – Ciudad Miguel Alemán, Tamaulipas



The United States is shown at the bottom of the photo. The Roma International Suspension Bridge is shown to the left of Roma-Ciudad Miguel Alemán Bridge.

LOCAL NAMES: Roma Suspension Bridge

LOCATION: TxDOT District: Pharr
Texas County: Starr
U.S. City: Roma
Mexican City: Ciudad Miguel Alemán

BRIDGE OWNER OR OPERATOR:
U.S.: Starr County
Mexico: Government of Mexico

YEAR OF CONSTRUCTION:
The Roma International Suspension Bridge in Starr County was constructed in 1928. Construction of a new bridge adjacent to the suspension bridge was completed in 1979. Vehicular and pedestrian use of the suspension bridge ended in 1978. Efforts are underway to rehabilitate the bridge for pedestrian crossing.

FUNDING/COST:

U.S.: Estimated construction costs are \$1.2 million for the U.S. portion and \$1.1 million for the Mexican portion. Through a Transportation Enhancement Project, the U.S. portion will be constructed using 80% federal funds and 20% local funds. Starr County and the City of Roma will fund the local 20% match as well as all costs above those approved under the Transportation Enhancement Program.

Source: Pharr District, TxDOT, August 1999

Since it is beyond TxDOT's statutory authority to perform contracted work outside the boundary of the United States, only the portion of the suspension bridge on the Texas side of the river is eligible for U.S. federal funds. However, state and federal dollars have been spent to perform the inspection, structure analysis and design on the entire structure.

Source: Pharr District, June 2001

Mexico: Approximately \$200,000 of the needed funds is available for the project.

Source: Centro S.C.T. Chihuahua, June 2001

IMPROVEMENTS:

This is the last of many suspension bridges built across the Rio Grande in the early 20th century and in recent years concerns have been raised regarding the bridge's structural integrity.

Source: Pharr District, TxDOT, August 1998.

Because of its unique construction and historical significance, many want to preserve the Roma International Suspension Bridge for pedestrian use. It is one of the historical sites in the Caminos del Rio Heritage Corridor jointly developed by Texas, U.S. and Mexican agencies. In 1993, the U.S. portion of the bridge was designated a National Historic Landmark -- the highest recognition for an historic property in the U.S.

Source: Texas Historical Commission, January 10, 1995

After negotiations at the April 2002 U.S.-Mexico Binational Bridges and Border Crossings Group Meeting, the U.S. State Department sent a Diplomatic note to the Mexican government extending the moratorium on the demolition of the bridge through June 2003. Negotiations continue.

Source: U.S. State Department, August 2002

Source: TxDOT, International Relations Office, February 2004

STATUS OF PROJECT:

U.S.: Plans for rehabilitation of the Mexican half of the bridge were completed and provided to Mexico in December 1998. Plans for the U.S. portion were completed in February 1999.

Source: Pharr District, TxDOT, August 1999

Mexico: The bridge has been declared a state historical landmark and national landmark recognition is being sought in order to receive federal funds for the rehabilitation.

Source: Centro S.C.T. Chihuahua, April 2002

La Linda Bridge
Brewster County, Texas - La Linda, Coahuila



The United States is shown at the bottom of the photo.

LOCAL NAMES: Big Bend Crossing Bridge, Puente La Linda, Heath Crossing

LOCATION: TxDOT District: El Paso
Texas County: Brewster
U.S. City: None
Mexican City: La Linda

DESCRIPTION: This is a single-lane structure.
Bridge length: 82 feet
Source: U.S. Department of State, March 1999
Bridge identification number-24 022 291301 001 01

BRIDGE OWNER OR OPERATOR:
U.S: Private ownership
Source: TxDOT, International Relations Office, January 2004
Mexico: Government of Mexico

YEAR OF CONSTRUCTION:
1965
Source: Bridge Inventory and Inspection File, TxDOT

HOURS:

The La Linda Bridge closed to land through traffic on July 30, 1997. The previous bridge owner, the National Parks and Conservation Association, had been granted an extension of a removal order if they could adequately demonstrate an economic plan of action which satisfied the concerns of the U.S. inspection services.

Source: U.S. Department of State, March 1999

The Government of Mexico responded via diplomatic note in December 1998 for a three-year extension of the removal order to launch an economic/eco-tourism feasibility study of the surrounding border area. An extension was granted until July 4, 2002. After negotiations at the April 2002 U.S.-Mexico Binational Bridges and Border Crossings Group Meeting, a diplomatic note was exchanged between the U.S. and Mexican governments extending the moratorium on the removal of the bridge through June 2003. The extension has expired, however negotiations continue between the U.S. and Mexico to resolve the moratorium issue.

Source: S.C.T., December 1998

Source: TxDOT, International Relations Office, February 2004

STATUS OF U.S. PERMITS AND MEXICAN APPROVALS:

U.S.: Presidential Permits are not required for bridges built before 1972.

BORDER STATION:

When the bridge was operational, the only border station was on the Mexican side and was owned by the Mexican government.

Source: El Paso District, TxDOT

CONNECTING ROADWAY:

U.S.: FM 2627 interconnects US 385.

Appendix III

Texas, U.S. and Mexican Bridge Approval Processes

STATE OF TEXAS APPROVAL PROCESS FOR CONSTRUCTING INTERNATIONAL BRIDGES BETWEEN TEXAS AND MEXICO

Senate Bill 1633, enacted by the 74th Texas Legislature (1995), requires a political subdivision or private entity to obtain approval from the Texas Transportation Commission (Commission) for the construction of the bridge before requesting a Presidential Permit. However, SB 1633 was amended by House Bill 1653 passed by the 78th Texas Legislature. HB 1653 allows an applicant to concurrently seek approval from the Commission and the U.S. However, if the Commission does not approve the construction of the bridge, the applicant shall withdraw the request for approval from the U.S.

TxDOT's Transportation Planning and Programming Division (TPP) is responsible for accepting and analyzing the applications submitted to the state by entities requesting permission to construct an international bridge across the Rio Grande. In making its determination, TPP consults with various divisions within TxDOT and various other agencies the Commission deems appropriate. Factors that are considered by the Commission include the local sponsor's financial resources, whether the bridge is consistent with the state and regional transportation plans, the facility's potential impact on the economy of the region, the environment, traffic congestion and free flow of trade.

The process is initiated when the potential applicant(s) request(s) the standardized application forms from TxDOT. Immediately upon receipt of the completed application and the requisite 20 copies, TPP forwards one copy of the application to the designated points of contact at the following TxDOT divisions and offices to determine if the application is complete:

- ◆ Design Division (DES)
- ◆ Environmental Affairs Division (ENV)
- ◆ Finance Division (FIN)
- ◆ Right of Way Division (ROW)
- ◆ International Relations Office (IRO)

If the application is deemed incomplete, it is returned and the process is repeated when the application is resubmitted. Once it has been determined that the application is complete or that the resubmitted application is no longer deficient, TPP notifies the applicant and the Governor's Office, in writing, that the application meets the requirements of Title 43 TAC, Section 15.74 and begins the analysis.

TPP is responsible for the following actions:

- ◆ Sends a copy of the application to the following agencies and local government entities, requesting their comments:
 - Department of Public Safety
 - Commission on Environmental Quality
 - Department of Agriculture
 - Historical Commission
 - Alcoholic Beverage Commission
 - Texas Department of Economic Development;
 - General Land Office; and
 - Other agencies the Commission deems appropriate.
- ◆ Sends a copy of the application to the Governor's Office, requesting comments.
- ◆ Requests analysis and the written results of that analysis from each TxDOT division and office mentioned above.

- ◆ Sends an application and requests analysis and the written results of that analysis from the appropriate district(s) and Metropolitan Planning Organization(s) (MPOs).
- ◆ Coordinates with TxDOT's General Counsel Office (OGC) to accomplish the following:
 - Schedule public hearing
 - Advertise public hearing
 - Conduct public hearing
- ◆ Compiles and summarizes public hearing comments.
- ◆ Analyzes compliance with the state transportation plan and, if appropriate, with the regional transportation plan developed by the metropolitan planning organization having jurisdiction over the project.
- ◆ Compiles and summarizes responses from state agencies, division, district(s), MPOs and /or local government entities.
- ◆ Prepares and sends staff response along with recommendation for commission action to the Executive Director through the Deputy Executive Director for Transportation Planning and Development.
- ◆ Coordinates with OGC to prepare documents and include on the commission meeting agenda recommended action.
- ◆ Notifies applicants and the Governor's Office in writing of commission action.

For more detailed information about the TxDOT permit process, please contact the TxDOT Transportation Planning and Programming Division at (512) 486-5038.

FEDERAL APPROVAL PROCESS FOR CONSTRUCTING INTERNATIONAL BRIDGES BETWEEN the UNITED STATES and MEXICO

United States: The U.S. federal approval process begins with the application for a Presidential Permit. The process is based on the International Bridges Act of 1972, (33 U.S.C. 535 et seq.) and Executive Order (E.O.) 11423, 33 FR 11741 (1968) as amended by E.O. 12847, 58 FR 96 (1993). The law authorizes the President to issue permits for construction of international bridges and to issue a Presidential Permit for a bridge if construction is deemed to be in the national interest. The responsibility for issuing Presidential Permits has been delegated to the Department of State. Prior to the International Bridges Act, approval to construct an international bridge was granted by individual Acts of Congress.

The Presidential Permit process involves the collaboration of both federal and Texas State agencies and may take several years due to environmental and other issues involved. Some of the federal agencies participating in this permitting process are: Department of Homeland Security (U.S. Customs and Border Protection and the U.S. Coast Guard), Food and Drug Administration, Department of Transportation (Federal Highway Administration and Federal Railroad Administration), Department of Commerce, Environmental Protection Agency, Department of the Interior (U.S. Fish and Wildlife Service), and the Department of Defense. These agencies are invited by the Department of State to comment on the permit application and on the environmental and other documentation submitted by the sponsor.

The Secretary of State or the Secretary's designee, after receiving the comments of the various agencies and after resolving questions that may arise during the consultative process, decides whether or not the construction of the proposed bridge is in the national interest. If a Finding of No Significant Impact (FONSI) is made, the Department of State proceeds to issue a Presidential Permit.

After the Presidential Permit is issued, the sponsor must get a permit from the U.S. Coast Guard. The International Boundary and Water Commission (IBWC) must grant its approval as well. If the site of the proposed crossing is owned by the IBWC, the sponsor must obtain a license from the Commission. If the land is privately owned, the sponsor must get the IBWC's approval. Approval is based on the design criteria of the bridge and must meet the terms specified in the 1970 Boundary Treaty.

Once the United States and Mexican governments concur on the construction of an international bridge, how to move forward with construction and other matters are reached through an exchange of diplomatic notes. For further information about the Presidential Permit process contact the Coordinator for US-Mexico Border Affairs at (202) 647-8529.

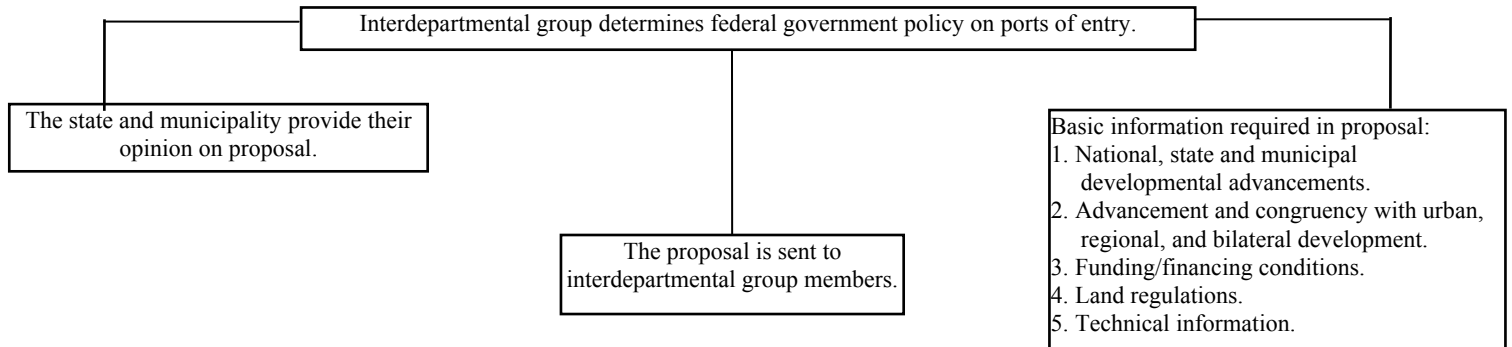
Source: US-Mexico Border Affairs, U.S. Department of State, March 1999

Mexico: The Mexican approval process for constructing international bridges is similar to that of the United States. The flowcharts on the following pages provide the detailed procedures and requirements.

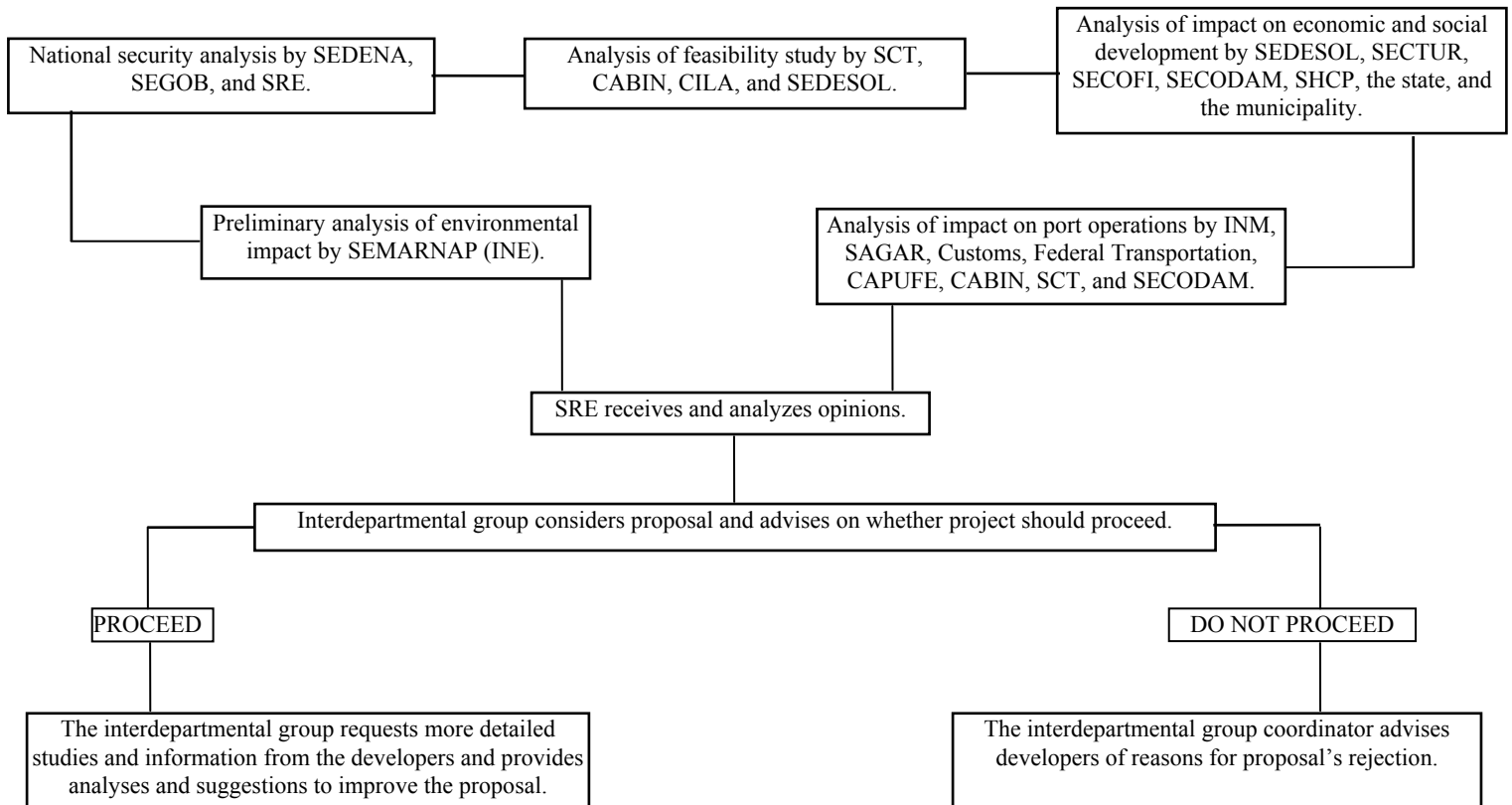
Source: SCT, February 1999

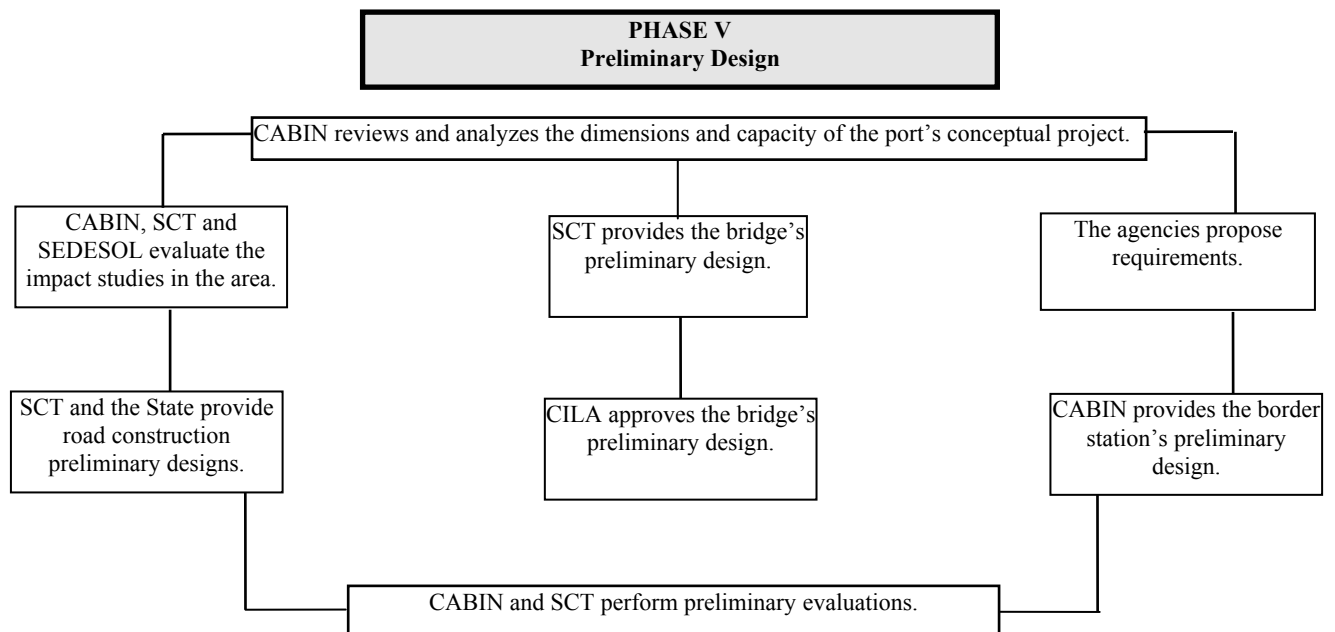
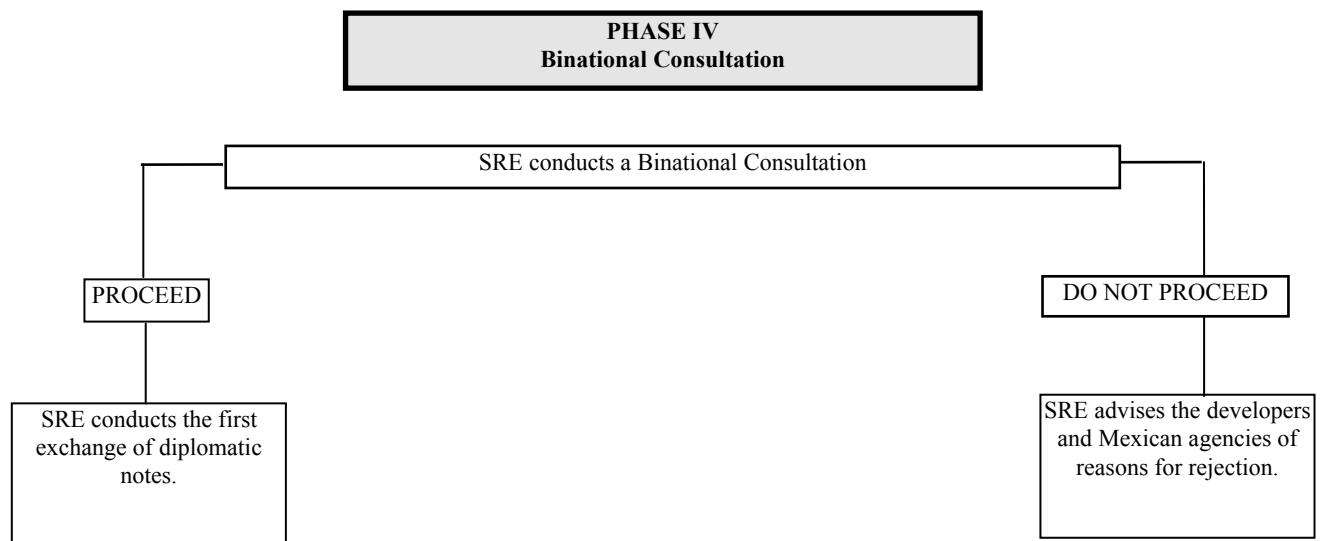
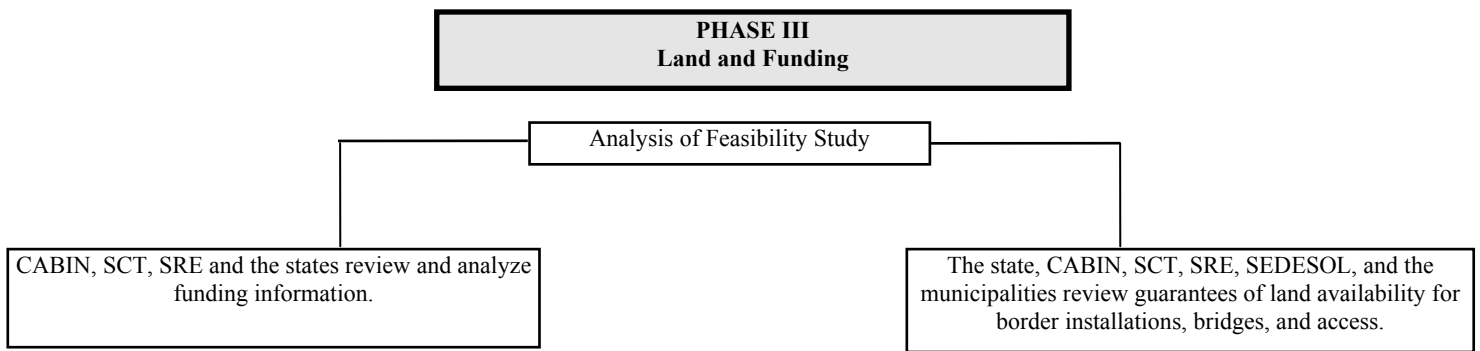
MEXICO'S COORDINATION PROCESS FOR ESTABLISHING NEW PORTS OF ENTRY

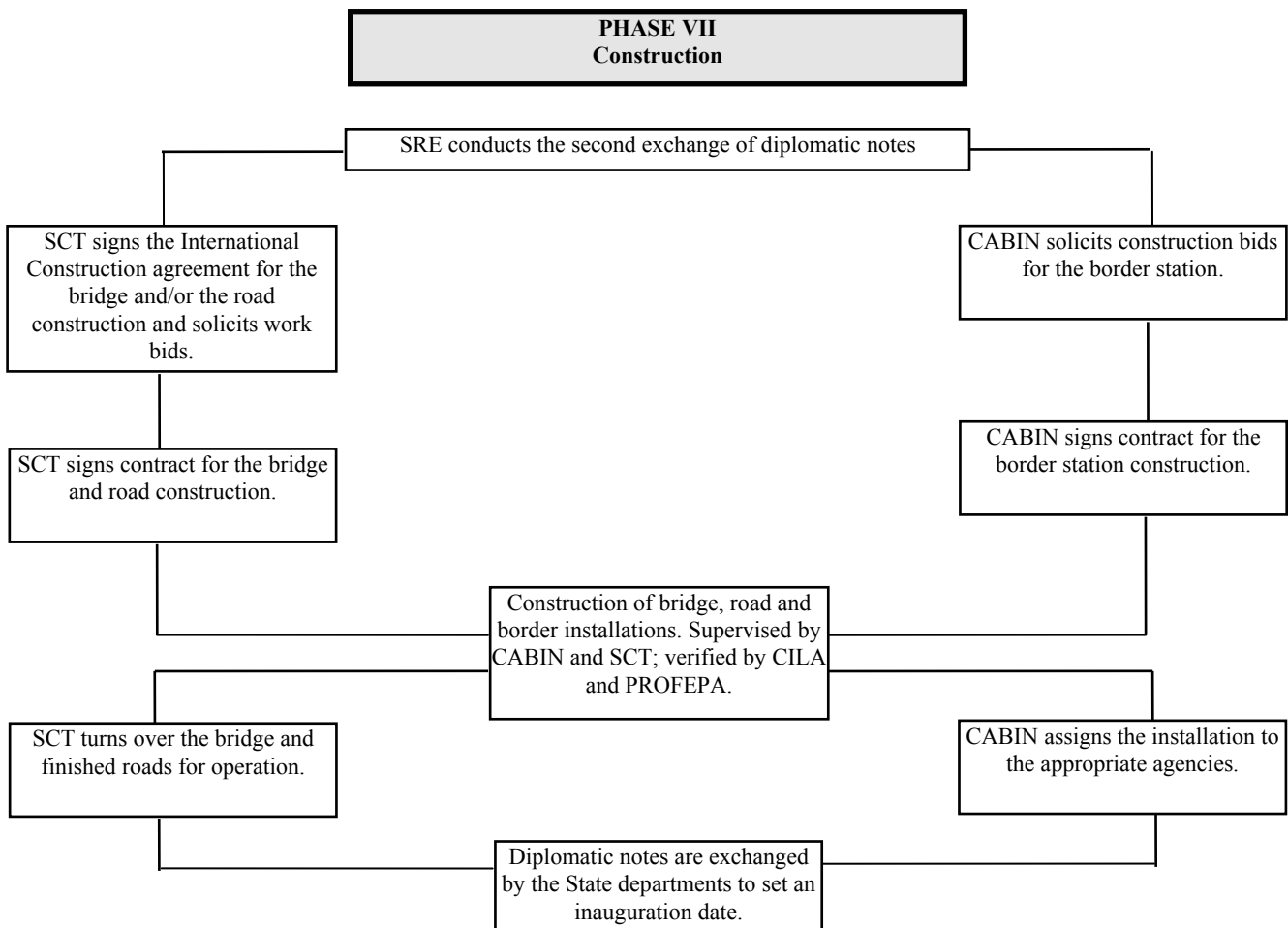
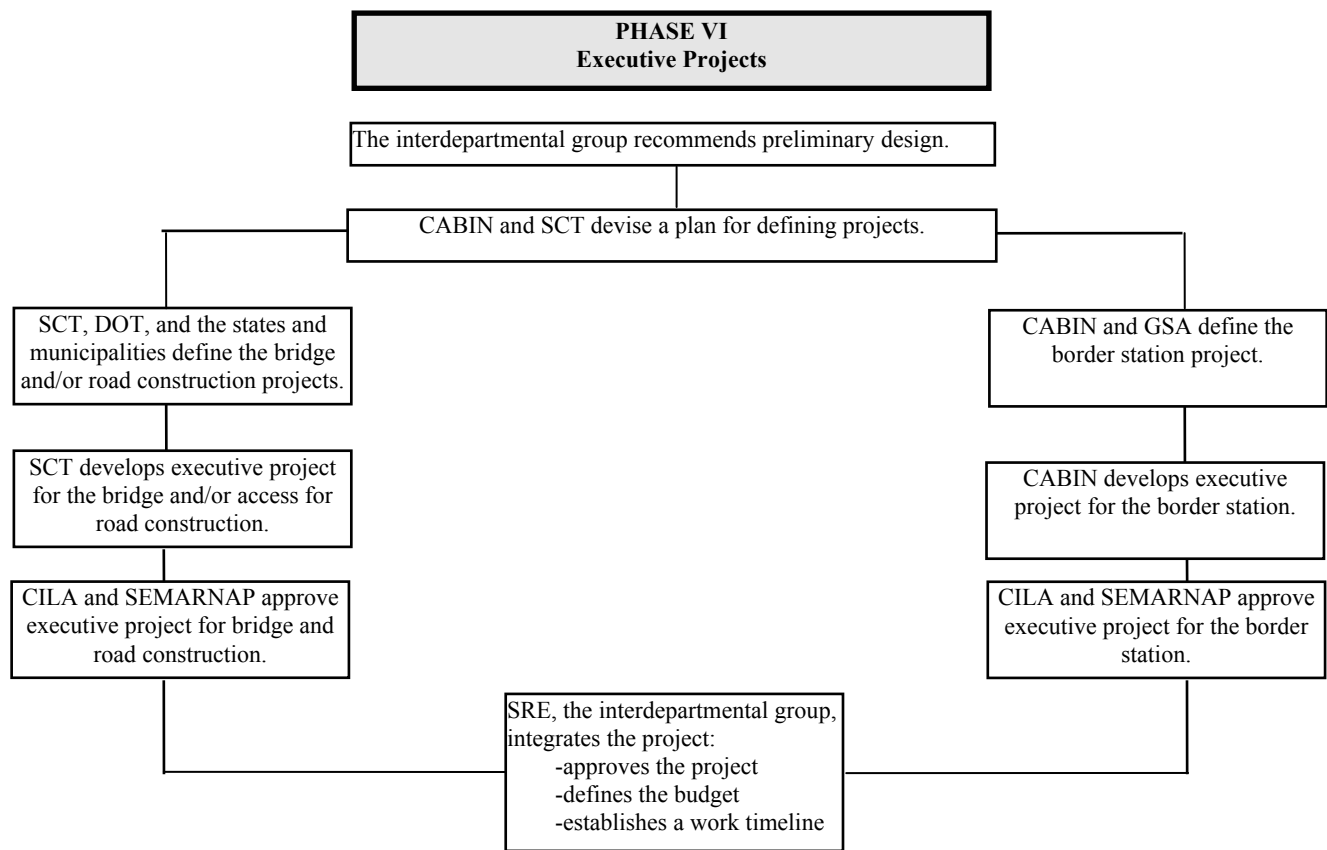
PHASE I Receipt and Distribution of Requests for New Ports of Entry



PHASE II Preliminary Technical Analysis







Appendix IV Traffic Counts 2002 and 2003

2002 - NORTHBOUND TRUCK MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	16,479	15,056	16,884	17,802	17,853	15,692	17,254	18,629	16,386	18,147	14,873	14,389	199,444
Gateway International Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
B&M Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Free Trade Bridge	4,287	3,742	4,057	4,155	4,460	4,921	4,754	4,796	4,116	4,269	3,082	2,786	49,425
Progreso International Bridge	2,128	1,788	1,747	2,386	2,044	1,944	1,731	2,377	2,541	1,864	1,601	1,735	23,886
Pharr-Reynosa Intl. Bridge on the Rise	31,424	30,365	35,351	36,459	35,376	31,640	32,715	33,796	30,104	34,884	30,273	27,895	390,282
McAllen-Hidalgo-Reynosa Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Los Ebanos Ferry	0	0	0	0	0	0	0	0	0	0	0	0	0
Rio Grande City-Camargo Bridge	2,275	2,099	2,193	2,320	2,309	1,969	2,142	2,181	2,304	2,427	2,007	2,104	26,330
Roma-Ciudad Miguel Aleman Bridge	844	955	929	1,016	992	793	788	815	634	702	603	529	9,600
Lake Falcon Dam Crossing	38	22	25	10	23	17	42	51	32	31	40	22	353
Juarez-Lincoln Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Gateway to the Americas Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
World Trade Bridge	88,716	85,488	89,619	97,799	92,562	87,710	97,804	100,678	92,209	103,894	92,760	87,396	1,116,635
Laredo-Colombia Solidarity Bridge	26,968	27,173	29,143	31,651	31,193	26,520	26,607	25,993	23,722	27,154	26,123	22,771	325,018
Camino Real International Bridge	7,345	6,967	7,041	8,067	8,018	7,363	7,602	8,068	7,542	8,593	7,111	6,139	89,856
Eagle Pass Bridge I	0	0	0	0	0	0	0	0	0	0	0	0	0
Del Rio-Ciudad Acuna Intl. Bridge	5,487	5,529	5,716	6,903	7,269	6,371	6,184	6,158	6,160	6,916	5,237	4,109	72,039
Lake Amistad Dam Crossing	0	0	0	0	0	0	0	0	0	0	0	0	0
Presidio Bridge	741	770	796	571	480	410	405	438	408	428	517	641	6,605
Fort Hancock-El Porvenir Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Fabens-Caseta Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Ysleta-Zaragoza Bridge	27,904	27,089	26,826	23,635	22,794	27,254	27,110	28,467	29,749	34,338	30,348	24,442	329,956
Bridge of the Americas	30,206	29,772	31,315	36,303	34,890	32,785	32,530	34,720	28,701	32,203	28,259	23,559	375,243
Good Neighbor Bridge (SB only)	0	0	0	0	0	0	0	0	0	0	0	0	0
Paso del Norte Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	244,842	236,815	251,642	269,077	260,263	245,389	257,668	267,167	244,608	275,850	242,834	218,517	3,014,672

2002- NORTHBOUND BUSES MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	1,154	944	1,024	932	1,004	1,068	1,137	1,306	1,172	1,134	962	1,090	12,927
Gateway International Bridge	62	59	58	63	62	55	66	64	71	57	59	66	742
B&M Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Free Trade Bridge	15	28	17	7	4	5	8	12	8	3	9	4	120
Progreso International Bridge	42	40	52	41	28	34	30	9	34	42	447	23	822
Pharr-Reynosa Intl. Bridge on the Rise	44	62	60	23	19	108	94	84	50	95	27	69	735
McAllen-Hidalgo-Reynosa Bridge	2,514	2,116	2,477	2,642	2,610	2,438	3,394	2,680	2,658	2,524	2,261	2,903	31,217
Los Ebanos Ferry	0	0	0	0	0	0	0	0	0	0	0	0	0
Río Grande City-Camargo Bridge	3	0	0	0	0	0	0	0	0	0	0	0	3
Roma-Ciudad Miguel Aleman Bridge	224	207	273	193	198	129	212	231	180	176	218	267	2,508
Lake Falcon Dam Crossing	3	4	3	2	4	1	5	1	3	2	3	1	32
Juarez-Lincoln Bridge	3,052	2,652	3,279	3,047	3,048	3,190	3,458	3,372	3,077	3,125	3,208	4,306	38,814
Gateway to the Americas Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
World Trade Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Laredo-Colombia Solidarity Bridge	2	13	8	15	0	0	0	0	0	0	0	0	38
Camino Real International Bridge	62	47	57	56	67	74	139	71	65	69	34	21	762
Eagle Pass Bridge I	147	122	135	150	178	161	203	138	193	151	152	187	1,917
Del Rio-Ciudad Acuna Intl. Bridge	558	504	563	541	560	541	564	562	543	558	541	558	6,593
Lake Amistad Dam Crossing	0	0	0	0	0	0	0	0	0	0	0	0	0
Presidio Bridge	18	28	32	15	16	17	26	22	31	31	29	32	297
Fort Hancock-El Porvenir Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Fabens-Caseta Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Ysleta-Zaragoza Bridge	4	9	18	22	21	29	45	35	35	36	54	32	340
Bridge of the Americas	740	659	825	877	979	1,006	1,017	1,367	1,299	1,278	1,269	1,349	12,665
Good Neighbor Bridge (SB only)	0	0	0	0	0	0	0	0	0	0	0	0	0
Paso del Norte Bridge	1,594	2,043	2,516	2,437	1,767	1,083	897	940	1,228	1,401	1,732	1,663	19,301
Total	10,238	9,537	11,397	11,063	10,565	9,939	11,295	10,894	10,647	10,682	11,005	12,571	129,833

2002 - NORTHBOUND POV's MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	161,226	161,941	185,516	178,827	187,088	176,585	181,418	193,329	186,262	188,689	181,870	189,417	2,172,168
Gateway International Bridge	188,224	180,704	204,318	195,812	204,298	199,412	205,534	206,466	186,866	200,109	193,235	195,221	2,360,199
B&M Bridge	192,914	190,248	209,460	212,639	226,601	218,229	227,476	207,521	214,194	214,063	205,372	214,548	2,533,265
Free Trade Bridge	68,049	63,847	80,648	68,646	70,633	68,984	74,724	71,710	61,969	68,577	61,509	71,881	831,177
Progreso International Bridge	103,019	102,310	119,769	102,035	102,976	101,347	102,517	101,627	89,464	89,282	94,912	104,753	1,214,011
Pharr-Reynosa Intl. Bridge on the Rise	180,586	173,086	205,961	187,535	201,551	195,318	221,772	205,744	188,082	190,883	180,860	202,891	2,334,269
McAllen-Hidalgo-Reynosa Bridge	482,918	461,565	499,244	480,967	499,791	494,595	490,065	507,041	479,672	473,741	447,039	485,193	5,801,831
Los Ebanos Ferry	3,794	2,813	3,170	2,591	173	3,320	1,502	3,800	2,413	3,207	2,933	4,059	33,775
Rio Grande City-Camargo Bridge	58,241	54,163	66,833	58,711	62,252	58,698	58,494	57,875	55,888	54,904	57,313	63,302	706,674
Roma-Ciudad Miguel Aleman Bridge	100,687	95,781	109,592	101,883	105,248	98,108	99,591	97,075	95,616	97,483	101,193	113,834	1,216,091
Lake Falcon Dam Crossing	16,080	14,050	18,027	14,309	14,247	13,170	14,463	13,523	13,240	12,899	13,975	17,092	175,075
Juarez-Lincoln Bridge	401,387	389,608	432,930	440,131	418,977	453,295	438,921	448,675	416,774	405,585	399,360	431,788	5,077,431
Gateway to the Americas Bridge	131,295	101,851	136,958	113,312	133,372	199,412	142,542	135,020	129,296	129,296	133,062	140,014	1,625,430
World Trade Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Laredo-Colombia Solidarity Bridge	25,629	16,324	28,463	16,774	16,697	16,102	26,505	24,813	20,510	17,043	27,055	43,065	278,980
Camino Real International Bridge	178,912	166,183	197,735	184,952	193,642	191,804	192,517	191,124	181,458	181,989	188,805	222,733	2,271,854
Eagle Pass Bridge I	115,171	112,457	123,281	119,252	127,069	120,927	123,931	122,926	119,357	120,027	127,570	140,071	1,472,039
Del Rio-Ciudad Acuna Intl. Bridge	156,420	153,856	180,874	166,269	176,915	175,642	177,567	179,302	166,647	165,718	164,800	165,519	2,029,529
Lake Amistad Dam Crossing	4,495	4,601	5,914	5,621	4,692	2,069	4,660	4,655	6,358	5,573	6,867	9,695	65,200
Presidio Bridge	62,519	58,776	68,950	61,404	65,462	63,394	62,079	61,816	56,604	55,089	57,474	66,196	739,763
Fort Hancock-El Porvenir Bridge	16,974	14,319	18,002	15,790	14,822	15,699	16,186	16,231	14,987	14,053	13,996	14,576	185,635
Fabens-Caseta Bridge	46,093	43,217	51,863	47,427	49,542	48,214	48,793	43,630	43,627	42,564	42,193	43,185	550,348
Ysleta-Zaragoza Bridge	343,689	298,734	315,367	291,647	328,484	296,570	300,367	285,831	256,108	273,600	275,144	284,328	3,549,869
Bridge of the Americas	356,887	348,584	398,357	387,094	399,121	379,539	384,350	399,172	392,769	415,743	424,230	463,714	4,749,560
Good Neighbor Bridge (SB only)	0	0	66,422	88,599	96,535	90,151	92,313	106,525	107,889	126,123	118,396	111,881	1,004,834
Paso del Norte Bridge	308,953	274,604	303,226	307,016	318,468	299,771	294,061	310,291	288,624	296,927	319,264	337,006	3,658,211
Total	3,704,162	3,483,622	4,030,880	3,849,243	4,018,656	3,980,355	3,982,348	3,995,722	3,774,674	3,843,167	3,838,427	4,135,962	46,637,218

2002 - NORTHBOUND PEDESTRIAN MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	7,858	7,045	8,968	8,356	9,279	10,004	13,439	9,578	12,319	11,915	7,021	9,823	115,605
Gateway International Bridge	224,757	219,309	308,023	236,079	245,635	244,134	261,712	256,302	213,469	198,519	213,141	300,213	2,921,293
B&M Bridge	13,460	13,998	14,470	13,391	19,931	12,455	13,839	13,421	13,906	12,997	11,464	15,371	168,703
Free Trade Bridge	356	268	384	352	545	479	572	588	531	425	365	382	5,247
Progreso International Bridge	154,286	169,505	188,431	88,174	76,939	85,282	96,081	76,736	63,017	64,430	94,525	131,100	1,288,506
Pharr-Reynosa Intl. Bridge on the Rise	3,498	3,947	4,967	5,021	5,760	5,216	5,339	5,841	3,152	3,688	3,710	3,474	53,613
McAllen-Hidalgo-Reynosa Bridge	139,950	123,635	173,207	155,772	152,983	165,145	169,757	161,425	145,564	145,183	156,984	215,698	1,905,303
Los Ebanos Ferry	8,139	8,573	8,573	8,190	8,573	8,573	8,573	8,573	8,573	8,573	8,573	8,573	102,059
Rio Grande City-Camargo Bridge	2,182	2,310	2,983	2,527	2,595	2,624	2,663	1,965	2,058	1,943	1,790	2,098	27,738
Roma-Ciudad Miguel Aleman Bridge	18,584	22,240	23,617	19,190	20,244	17,060	27,799	25,489	16,483	15,678	17,570	21,422	245,376
Lake Falcon Dam Crossing	0	0	0	0	0	1	0	0	0	0	0	0	1
Juarez-Lincoln Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Gateway to the Americas Bridge	341,855	329,806	329,806	383,702	400,426	362,039	421,752	394,854	369,782	353,894	376,025	467,580	4,531,521
World Trade Bridge	8,382	7,548	8,066	8,802	7,431	6,462	9,884	9,727	8,510	8,478	8,558	8,033	99,881
Laredo-Colombia Solidarity Bridge	1,193	1,333	1,208	1,514	1,755	1,659	1,727	1,296	1,186	1,294	1,252	1,227	16,644
Camino Real International Bridge	3,694	2,886	4,087	3,710	3,954	2,461	6,370	3,425	2,779	2,158	1,976	2,220	39,720
Eagle Pass Bridge I	55,092	54,597	66,873	62,439	52,232	50,726	53,804	56,018	53,793	28,107	47,221	71,282	652,184
Del Rio-Ciudad Acuna Intl. Bridge	14,301	11,034	21,566	11,593	13,796	14,697	17,272	15,085	12,485	10,105	11,251	13,968	167,153
Lake Amistad Dam Crossing	0	0	0	0	0	0	0	0	0	0	0	0	0
Presidio Bridge	1,744	2,400	2,792	2,834	3,250	2,985	3,100	3,619	3,244	3,362	2,137	2,598	34,065
Fort Hancock-El Porvenir Bridge	163	172	242	289	266	192	180	198	160	162	207	199	2,430
Fabens-Caseta Bridge	2,448	2,827	3,394	2,394	1,686	2,316	3,575	2,826	2,960	2,417	2,189	2,260	31,292
Ysleta-Zaragoza Bridge	51,869	61,585	79,729	74,161	73,696	68,452	80,953	75,158	71,532	68,947	64,042	70,829	840,953
Bridge of the Americas	73,605	82,665	110,153	115,133	113,325	107,325	119,557	105,675	89,170	98,330	84,231	108,569	1,207,738
Good Neighbor Bridge (SB only)	0	0	0	0	0	0	0	0	0	0	0	0	0
Paso del Norte Bridge	563,708	462,463	543,192	482,371	653,326	641,327	677,829	676,351	610,752	619,403	624,804	696,465	7,251,991
Total	1,691,124	1,590,146	1,904,731	1,685,994	1,867,627	1,811,614	1,995,777	1,904,150	1,705,425	1,660,008	1,739,036	2,153,384	21,709,016

2003- NORTHBOUND TRUCK MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	16,120	14,636	16,084	16,006	15,982	14,800	16,401	16,554	15,738	17,252	15,171	14,757	189,319
Gateway International Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
B&M Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Free Trade Bridge	3,162	3,053	3,239	3,081	3,711	4,068	4,215	3,700	3,428	3,081	2,702	2,630	40,070
Progreso International Bridge	1,634	1,433	1,580	1,465	1,485	1,294	1,662	1,815	2,178	2,124	1,500	1,401	19,571
Pharr-Reynosa Intl. Bridge on the Rise	33,092	32,148	37,542	37,481	33,399	32,365	32,439	32,017	32,250	37,172	32,495	33,664	406,064
McAllen-Hidalgo-Reynosa Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Los Ebanos Ferry	0	0	0	0	0	0	0	0	0	0	0	0	0
Rio Grande City-Camargo Bridge	2,587	2,303	2,951	2,642	3,166	3,085	3,529	3,140	2,911	3,456	2,691	3,062	35,523
Roma-Ciudad Miguel Aleman Bridge	508	577	657	680	615	600	695	710	668	759	349	601	7,419
Lake Falcon Dam Crossing	23	33	33	24	14	16	23	7	9	5	21	6	214
Juarez-Lincoln Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Gateway to the Americas Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
World Trade Bridge	94,513	84,747	88,700	90,301	90,774	92,687	91,035	86,619	87,342	97,843	86,789	88,137	1,079,487
Laredo-Colombia Solidarity Bridge	23,260	23,040	25,083	22,646	23,439	22,097	23,977	22,738	23,375	24,711	20,461	19,915	274,742
Camino Real International Bridge	7,156	6,811	7,475	7,422	7,524	7,401	7,122	7,049	7,197	8,235	7,341	7,539	88,272
Eagle Pass Bridge I	0	0	0	0	0	0	0	0	0	0	0	0	0
Del Rio-Ciudad Acuna Intl. Bridge	6,014	5,730	5,808	5,856	5,895	6,054	4,272	5,800	4,656	6,295	5,238	3,991	65,609
Lake Amistad Dam Crossing	0	0	0	0	0	0	0	0	0	0	0	0	0
Presidio Bridge	505	377	483	543	379	320	373	331	394	466	839	710	5,720
Fort Hancock-El Porvenir Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Fabens-Caseta Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Ysleta-Zaragoza Bridge	30,542	26,895	24,268	21,741	26,684	25,722	24,814	25,105	26,916	29,457	26,876	24,698	313,718
Bridge of the Americas	25,100	26,912	28,590	27,713	29,555	28,793	27,784	29,882	31,224	34,238	28,840	27,265	345,896
Good Neighbor Bridge (SB only, NB DCL)	0	0	0	0	0	0	0	0	0	0	0	0	0
Paso del Norte Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0

Total	244,216	228,695	242,493	237,601	242,622	239,302	238,341	235,467	238,286	265,094	231,313	228,194	2,871,624
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2003- NORTHBOUND BUSES MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	1,063	904	1,029	972	963	1,059	924	948	831	800	798	943	11,234
Gateway International Bridge	56	56	55	49	56	43	5	2	4	1	0	8	335
B&M Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Free Trade Bridge	8	19	21	10	5	7	3	7	6	8	6	4	104
Progreso International Bridge	33	32	47	36	29	38	28	10	21	52	61	22	409
Pharr-Reynosa Intl. Bridge on the Rise	36	44	73	39	24	112	161	78	88	31	80	31	797
McAllen-Hidalgo-Reynosa Bridge	2,731	3,453	2,874	2,178	2,674	2,522	2,513	2,454	2,605	2,779	2,734	2,491	32,008
Los Ebanos Ferry	0	0	0	0	0	0	0	0	0	0	0	0	0
Rio Grande City-Camargo Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Roma-Ciudad Miguel Aleman Bridge	188	167	203	183	166	175	179	187	135	131	150	182	2,046
Lake Falcon Dam Crossing	2	4	1	0	0	0	0	0	0	0	0	1	8
Juarez-Lincoln Bridge	2,954	2,497	3,048	2,644	2,822	2,976	3,328	3,030	2,721	2,776	2,964	3,536	35,296
Gateway to the Americas Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
World Trade Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Laredo-Colombia Solidarity Bridge	0	1	3	43	7	43	7	2	0	2	2	0	110
Camino Real International Bridge	29	23	25	14	7	31	25	14	27	20	30	43	288
Eagle Pass Bridge I	175	157	153	114	0	15	110	144	99	139	130	167	1,403
Del Rio-Ciudad Acuna Intl. Bridge	560	508	560	541	558	543	560	342	0	1	0	5	4,178
Lake Amistad Dam Crossing	0	0	0	0	0	0	0	0	0	0	0	0	0
Presidio Bridge	28	24	27	24	28	11	23	21	30	27	19	37	299
Fort Hancock-El Porvenir Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Fabens-Caseta Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Ysleta-Zaragoza Bridge	21	31	18	32	39	76	66	42	45	59	72	41	542
Bridge of the Americas	1,196	1,203	1,174	1,182	1,404	1,426	1,075	1,058	854	847	864	1,068	13,351
Good Neighbor Bridge (SB only, NB DCL)	0	0	0	0	0	0	0	0	0	0	0	0	0
Paso del Norte Bridge	1,737	1,253	1,394	1,268	1,340	1,262	1,162	1,357	1,241	1,365	1,332	1,427	16,138
Total	10,817	10,376	10,705	9,329	10,122	10,339	10,169	9,696	8,707	9,038	9,242	10,006	118,546

2003- NORTHBOUND POV's MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	175,573	157,237	176,190	175,983	186,416	169,343	168,406	168,980	154,709	161,974	164,215	176,008	2,035,034
Gateway International Bridge	180,203	163,429	185,344	184,093	203,179	192,264	204,734	199,463	184,914	191,096	190,796	208,835	2,288,350
B&M Bridge	188,175	174,469	196,544	183,387	194,387	164,357	176,647	180,825	162,891	146,003	175,643	196,617	2,139,945
Free Trade Bridge	60,223	55,014	65,172	61,758	66,933	63,406	67,976	64,136	56,346	57,400	64,636	73,536	756,536
Progreso International Bridge	95,877	92,942	100,813	94,678	98,342	92,654	95,648	92,669	84,984	90,912	100,240	111,415	1,151,174
Pharr-Reynosa Intl. Bridge on the Rise	167,179	158,419	176,887	178,392	197,982	189,030	195,372	182,392	167,946	180,323	181,023	192,328	2,167,273
McAllen-Hidalgo-Reynosa Bridge	450,930	391,263	418,910	410,967	426,968	406,027	425,520	436,124	383,705	386,533	405,050	460,359	5,002,356
Los Ebanos Ferry	3,031	2,949	3,507	1,758	1,304	3,340	3,157	3,169	1,774	1,493	3,058	3,126	31,666
Rio Grande City-Camargo Bridge	56,072	52,446	59,635	57,557	59,905	55,717	52,820	55,567	51,021	53,288	57,849	62,679	374,556
Roma-Ciudad Miguel Aleman Bridge	98,823	88,851	100,595	94,824	98,063	89,500	80,709	91,588	82,402	86,832	93,822	102,976	1,108,985
Lake Falcon Dam Crossing	14,587	12,483	14,914	14,028	12,646	11,719	12,368	11,553	10,770	10,985	13,410	14,705	154,168
Juarez-Lincoln Bridge	417,780	367,040	397,802	360,685	422,182	398,125	413,696	420,766	398,517	400,213	394,195	406,610	4,797,611
Gateway to the Americas Bridge	129,372	114,410	137,256	149,151	150,183	149,045	177,012	145,634	140,201	149,461	146,387	150,721	1,738,833
World Trade Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Laredo-Colombia Solidarity Bridge	24,120	17,116	22,536	21,711	16,039	14,893	20,706	18,348	13,736	12,356	23,415	36,003	240,979
Camino Real International Bridge	178,886	161,685	183,403	178,741	184,113	172,586	174,778	172,468	152,515	160,661	169,322	195,524	2,084,682
Eagle Pass Bridge I	125,870	114,060	130,297	121,928	126,893	120,208	123,744	121,272	116,532	123,088	123,728	141,349	1,488,969
Del Rio-Ciudad Acuna Intl. Bridge	149,261	133,719	157,110	150,388	158,957	155,496	154,638	156,824	145,222	150,010	148,824	159,063	1,819,512
Lake Amistad Dam Crossing	6,344	7,901	8,451	7,219	6,810	6,012	7,740	7,707	6,337	6,624	8,747	10,235	90,127
Presidio Bridge	58,931	51,364	60,486	57,139	59,324	59,675	60,566	59,257	54,534	54,961	60,436	65,248	701,921
Fort Hancock-El Porvenir Bridge	13,923	13,881	15,065	14,107	14,291	13,108	13,197	13,425	12,920	12,771	13,761	13,998	164,447
Fabens-Caseta Bridge	42,710	38,617	43,593	42,508	44,069	38,210	36,049	39,712	40,949	39,556	43,320	45,091	494,384
Ysleta-Zaragoza Bridge	285,092	232,251	282,804	281,950	317,047	308,794	308,960	315,668	299,864	270,293	184,613	282,708	3,370,044
Bridge of the Americas	353,626	376,067	347,249	318,751	348,662	376,142	355,793	400,629	364,387	497,285	486,611	454,569	4,679,771
Good Neighbor Bridge (SB only, NB DCL)	121,955	118,158	125,438	136,682	102,546	109,301	115,331	123,976	135,675	137,281	123,176	125,851	1,475,370
Paso del Norte Bridge	327,094	278,710	346,185	351,739	376,250	365,736	377,308	375,529	368,494	337,394	319,914	348,912	4,173,265
Total	3,725,637	3,374,481	3,756,186	3,650,124	3,873,491	3,724,688	3,822,875	3,857,681	3,591,345	3,718,793	3,696,191	4,038,466	44,829,958

2003- NORTHBOUND PEDESTRIAN MONTHLY TRAFFIC FIGURES

Source: U.S. Customs and Border Protection

POE	January	February	March	April	May	June	July	August	September	October	November	December	Total
Veterans International Bridge	9,242	6,894	7,673	8,400	7,880	8,902	12,941	10,687	7,760	7,581	7,957	11,774	107,691
Gateway International Bridge	203,954	198,320	250,100	223,539	222,352	215,734	232,051	217,447	199,595	201,875	213,404	275,288	2,653,659
B&M Bridge	12,678	10,988	12,271	11,702	13,128	13,250	15,204	14,138	11,049	11,700	12,448	16,294	154,850
Free Trade Bridge	311	330	368	267	362	433	456	400	350	320	285	273	4,155
Progreso International Bridge	146,395	159,958	178,368	87,033	76,115	82,484	94,038	79,437	59,005	74,295	103,274	135,479	1,275,881
Pharr-Reynosa Intl. Bridge on the Rise	3,090	3,681	3,443	3,752	4,984	4,563	4,718	2,754	4,950	2,486	4,325	4,458	47,204
McAllen-Hidalgo-Reynosa Bridge	143,624	158,854	267,895	159,205	164,677	160,432	174,536	163,343	159,456	158,641	168,480	211,885	2,091,028
Los Ebanos Ferry	8,573	7,145	8,573	8,573	8,573	8,573	8,573	8,573	8,573	8,573	8,573	8,573	101,448
Rio Grande City-Camargo Bridge	1,761	2,173	1,973	1,539	1,239	1,443	1,462	1,567	1,543	1,566	1,497	1,938	19,701
Roma-Ciudad Miguel Aleman Bridge	17,434	15,693	20,334	21,098	20,205	16,571	21,891	24,947	20,737	17,829	20,282	25,373	242,394
Lake Falcon Dam Crossing	0	0	0	0	0	0	0	0	0	0	0	0	0
Juarez-Lincoln Bridge	0	0	0	0	0	0	0	0	0	0	0	0	0
Gateway to the Americas Bridge	358,280	339,639	386,790	374,154	399,962	366,575	390,123	373,475	302,603	360,027	369,848	437,806	4,459,282
World Trade Bridge	8,221	8,706	8,299	8,198	9,491	8,841	9,572	8,997	8,106	8,126	8,812	8,954	104,323
Laredo-Colombia Solidarity Bridge	1,426	1,476	1,199	1,090	1,080	1,083	1,343	1,257	1,188	1,235	955	788	14,120
Camino Real International Bridge	1,833	1,671	2,106	1,912	1,858	2,508	3,141	1,937	2,091	2,099	2,433	2,676	26,265
Eagle Pass Bridge I	53,199	49,031	58,327	59,388	51,710	42,612	60,365	57,449	52,501	53,744	58,847	75,164	672,337
Del Rio-Ciudad Acuna Intl. Bridge	11,998	10,594	15,579	11,556	10,342	12,045	11,992	9,648	6,696	7,349	8,857	15,560	132,216
Lake Amistad Dam Crossing	0	0	0	0	0	0	0	0	0	0	0	0	0
Presidio Bridge	2,866	2,535	3,257	3,142	1,746	1,750	1,565	1,522	1,681	1,480	1,655	1,958	25,157
Fort Hancock-El Porvenir Bridge	172	190	142	97	92	102	70	95	88	109	89	82	1,328
Fabens-Caseta Bridge	1,847	3,303	2,410	2,137	1,862	1,775	1,639	1,861	1,535	1,354	1,726	2,534	23,983
Ysleta-Zaragoza Bridge	56,886	69,872	68,310	73,163	66,798	59,996	72,399	68,992	65,180	62,095	61,295	72,471	797,457
Bridge of the Americas	85,477	94,390	92,530	99,644	93,573	86,200	89,581	82,838	72,102	65,338	68,335	91,285	1,021,293
Good Neighbor Bridge (SB only, NB DCL)	0	0	0	0	0	0	0	0	0	0	0	0	0
Paso del Norte Bridge	555,479	500,251	561,104	577,694	610,482	572,040	620,032	634,685	572,501	569,645	588,139	717,393	7,079,445
Total	1,684,746	1,645,694	1,951,051	1,737,283	1,768,511	1,667,912	1,827,692	1,766,049	1,559,290	1,617,467	1,711,516	2,118,006	21,055,217

Appendix V

Detailed Area Maps

LEGEND/MAPS

I. Existing Bridges

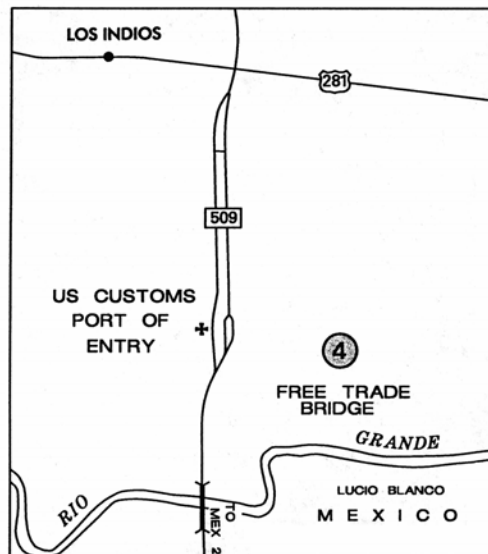
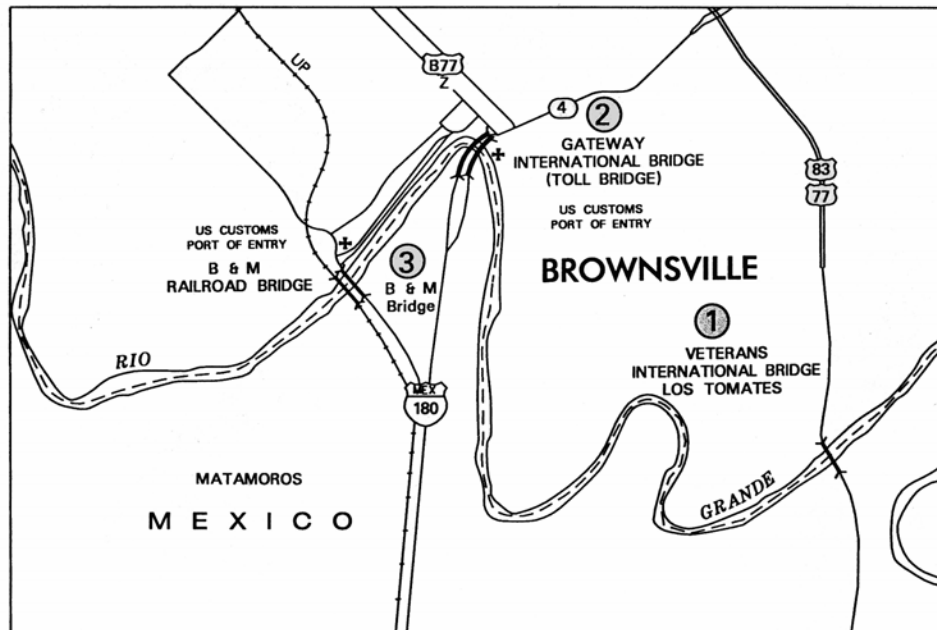
- 1) Veterans Intl. Bridge at Los Tomates
- 2) Gateway International Bridge
- 3) B & M Bridge
- 4) Free Trade Bridge
- 5) Progreso International Bridge
- 6) Pharr-Reynosa International Bridge on the Rise
- 7) McAllen-Hidalgo-Reynosa Bridge
- 8) Los Ebanos Ferry
- 9) Rio Grande City-Camargo Bridge
- 10) Roma-Ciudad Miguel Alemán Bridge
- 11) Lake Falcon Dam Crossing
- 12) Juárez-Lincoln Bridge
- 13) Gateway to the Americas Bridge
- 14) World Trade Bridge
- 15) Laredo-Colombia Solidarity Bridge
- 16) Camino Real International Bridge
- 17) Eagle Pass Bridge I
- 18) Del Río-Ciudad Acuña International Bridge
- 19) Lake Amistad Dam Crossing
- 20) Presidio Bridge
- 21) Fort Hancock-El Porvenir Bridge
- 22) Fabens-Caseta Bridge
- 23) Ysleta-Zaragoza Bridge
- 24) Bridge of the Americas (BOTA)
- 25) Good Neighbor Bridge
- 26) Paso del Norte Bridge

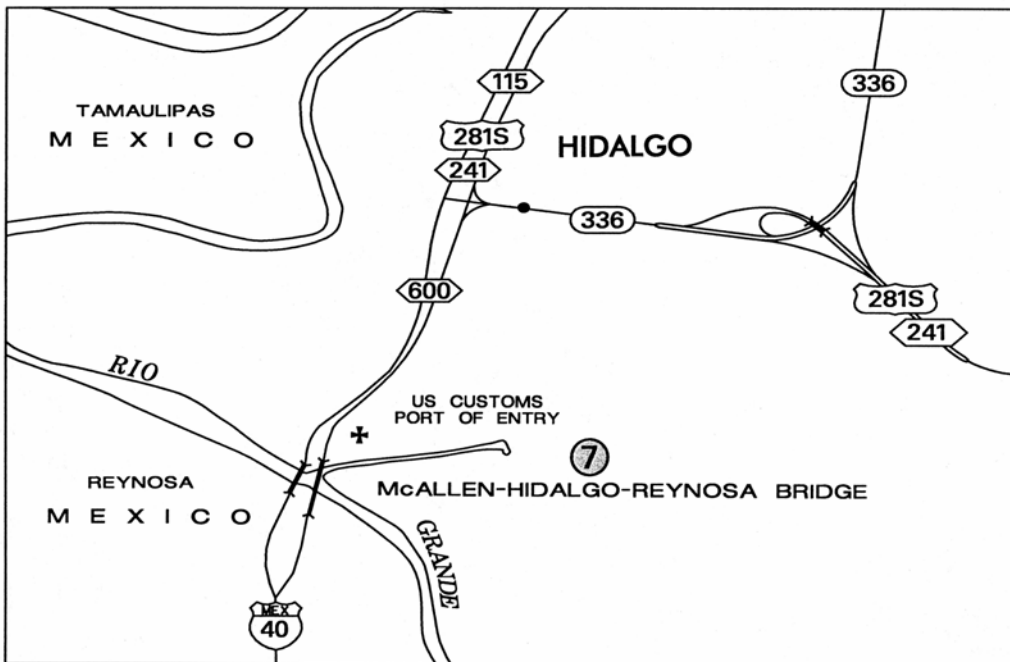
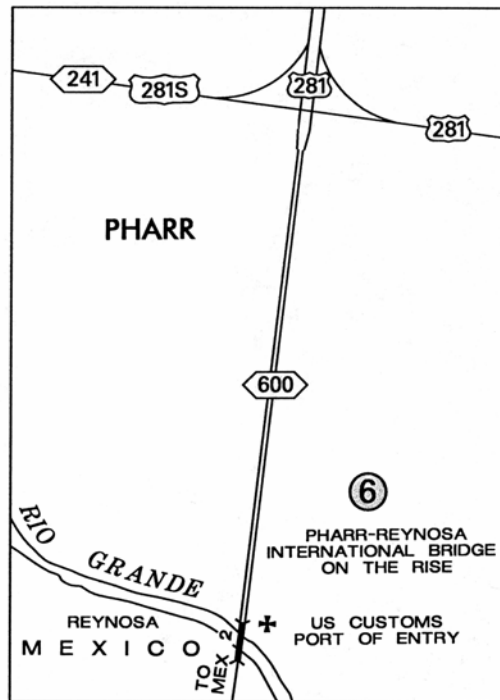
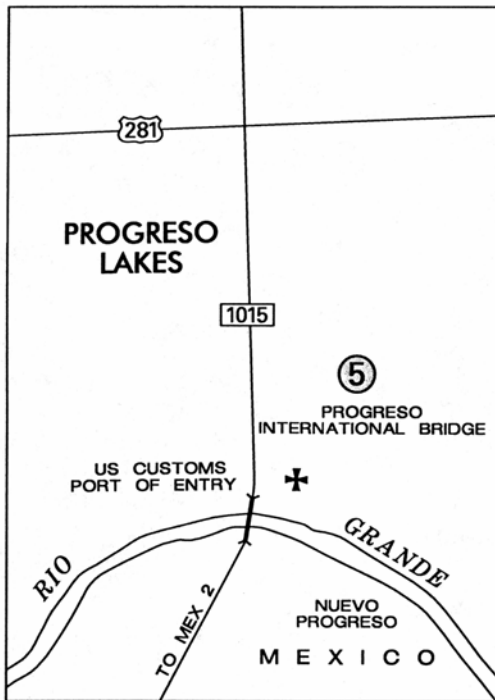
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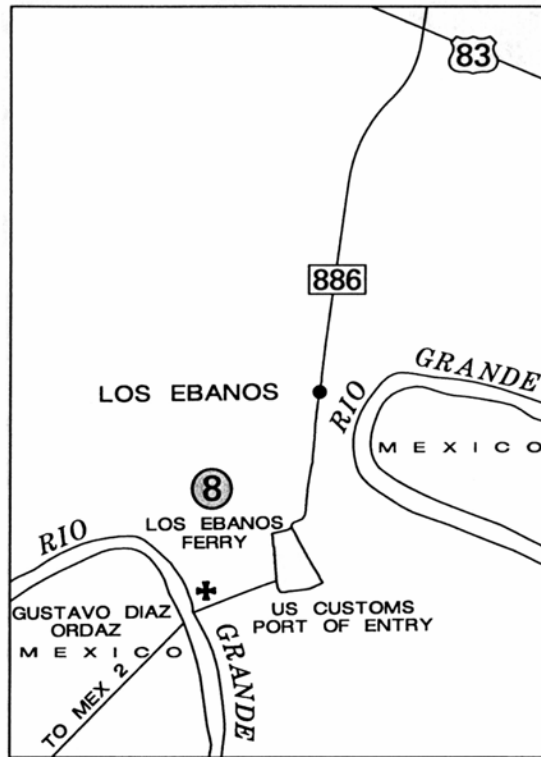
- A. Port of Brownsville Bridge
- B. Donna International Bridge (no map available)
- C. Anzalduas International Bridge
- D. Los Ebanos International Bridge (no map available)
- E. Mission International Bridge (no map available)
- F. Laredo V International Bridge (no map available)
- G. Guadalupe-Tornillo International Bridge (See #22 above; new bridge will be 650 yards upstream from existing Fabens-Caseta Bridge)

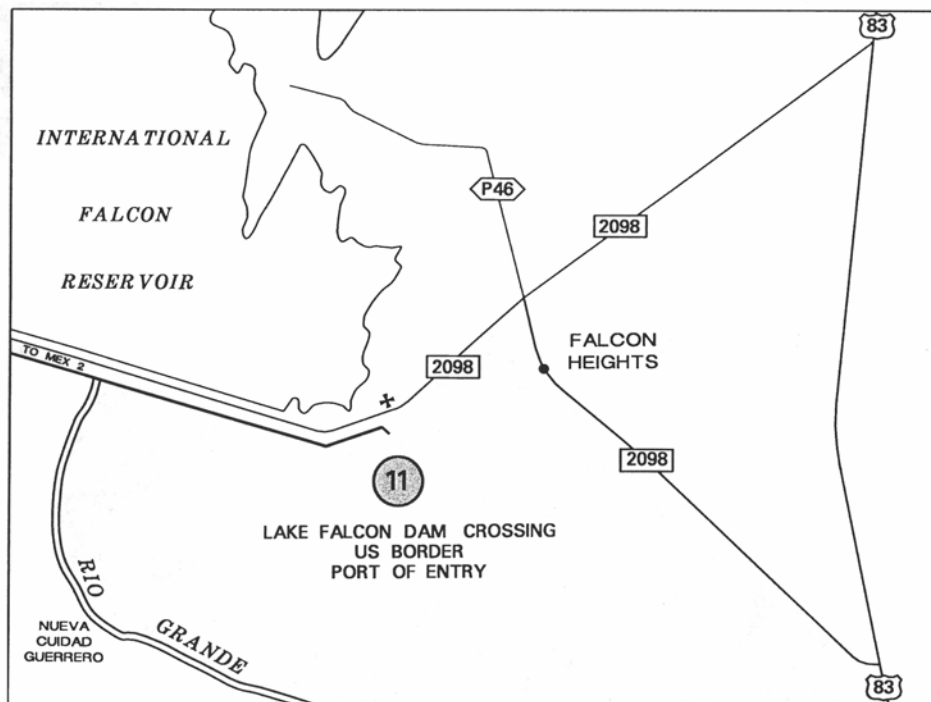
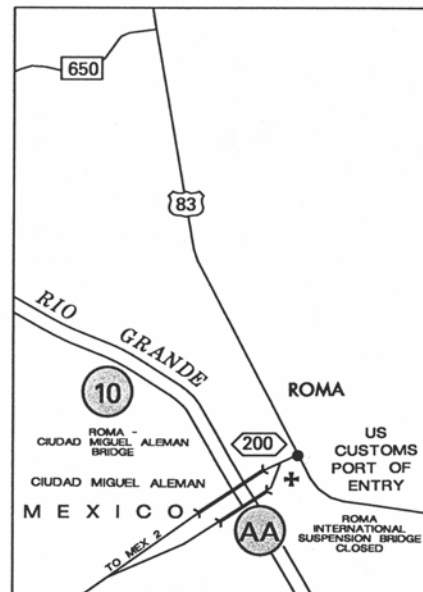
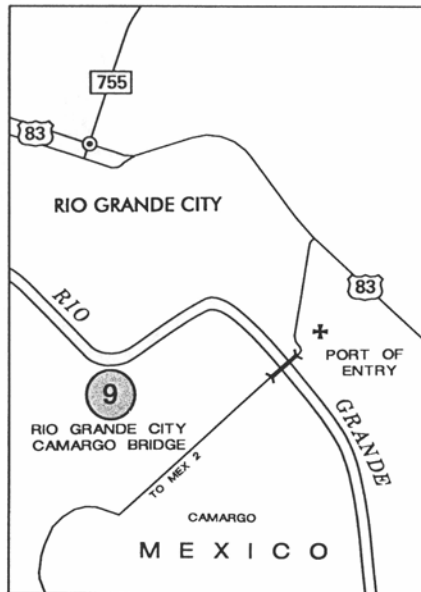
III. Closed Bridges

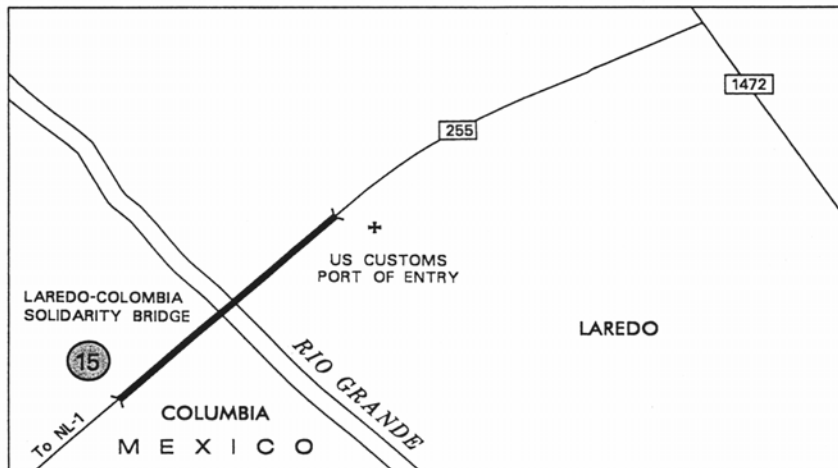
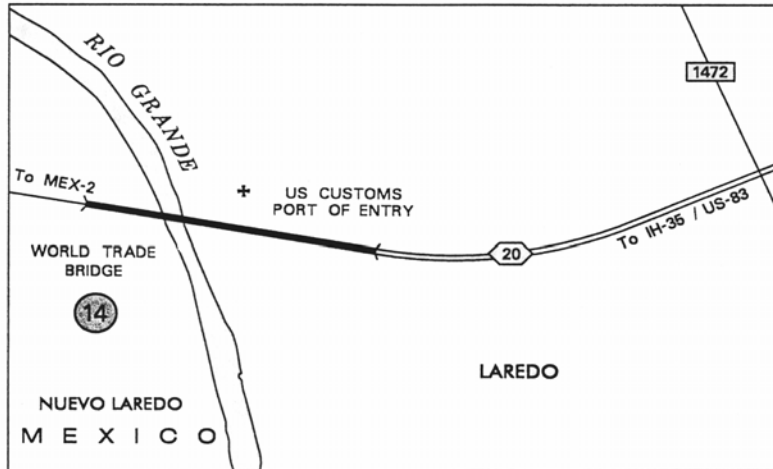
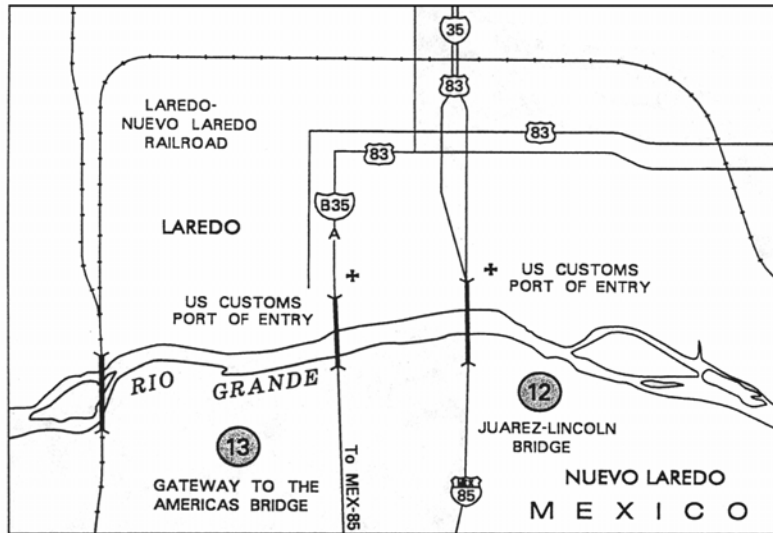
- AA. Roma International Suspension Bridge (Rehabilitation Pending)
- BB. La Linda Bridge

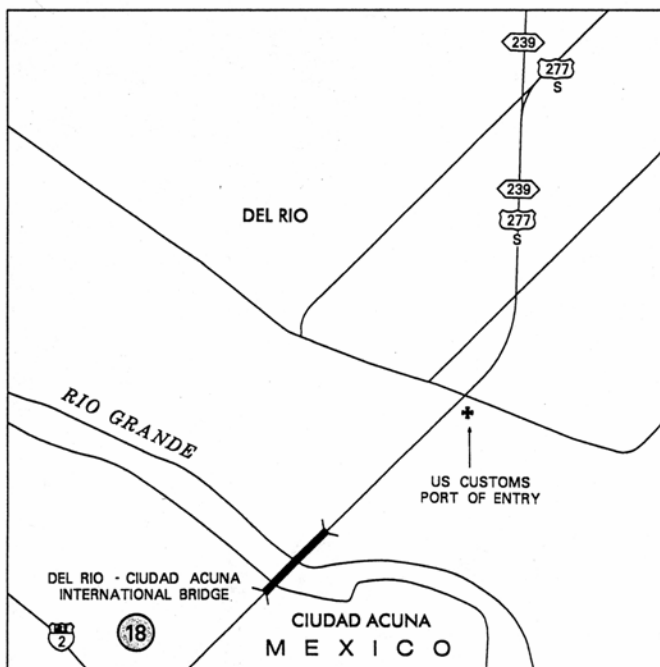
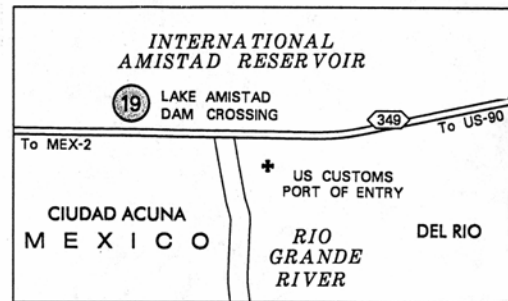
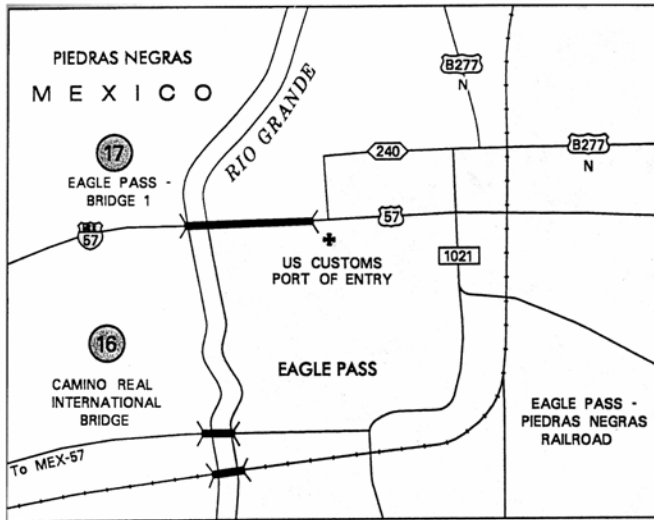


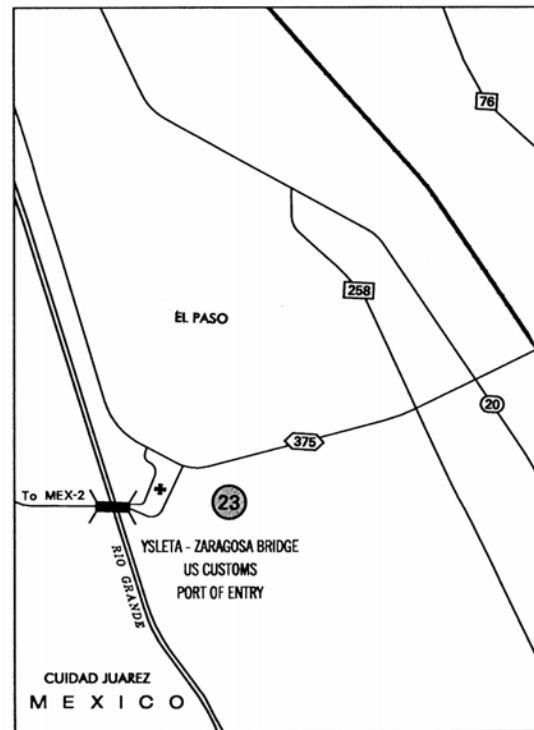
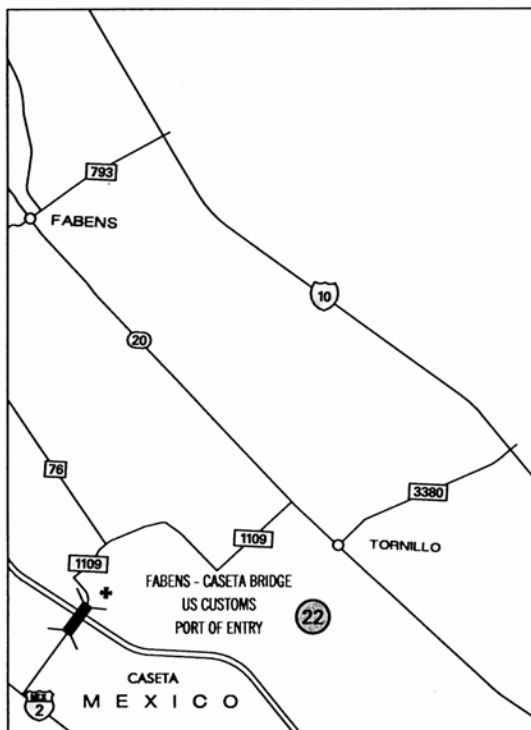
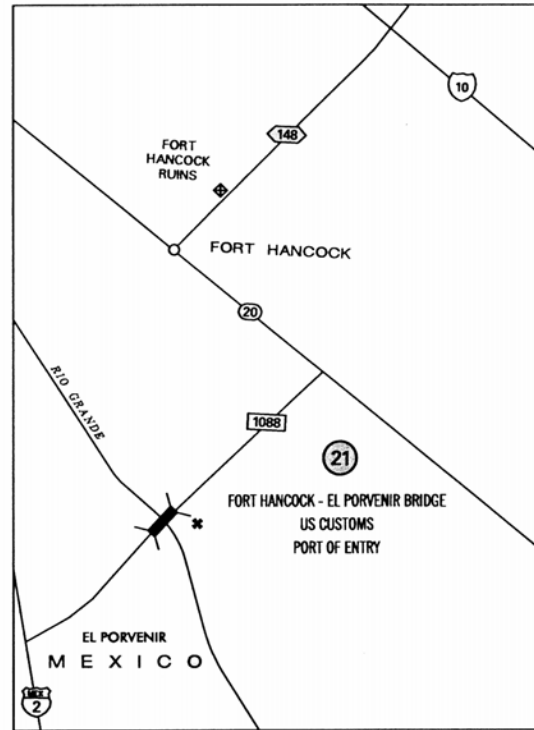
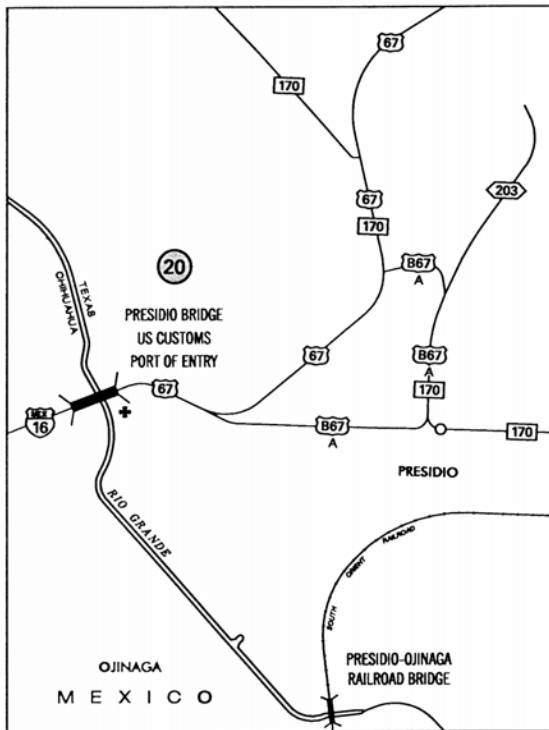


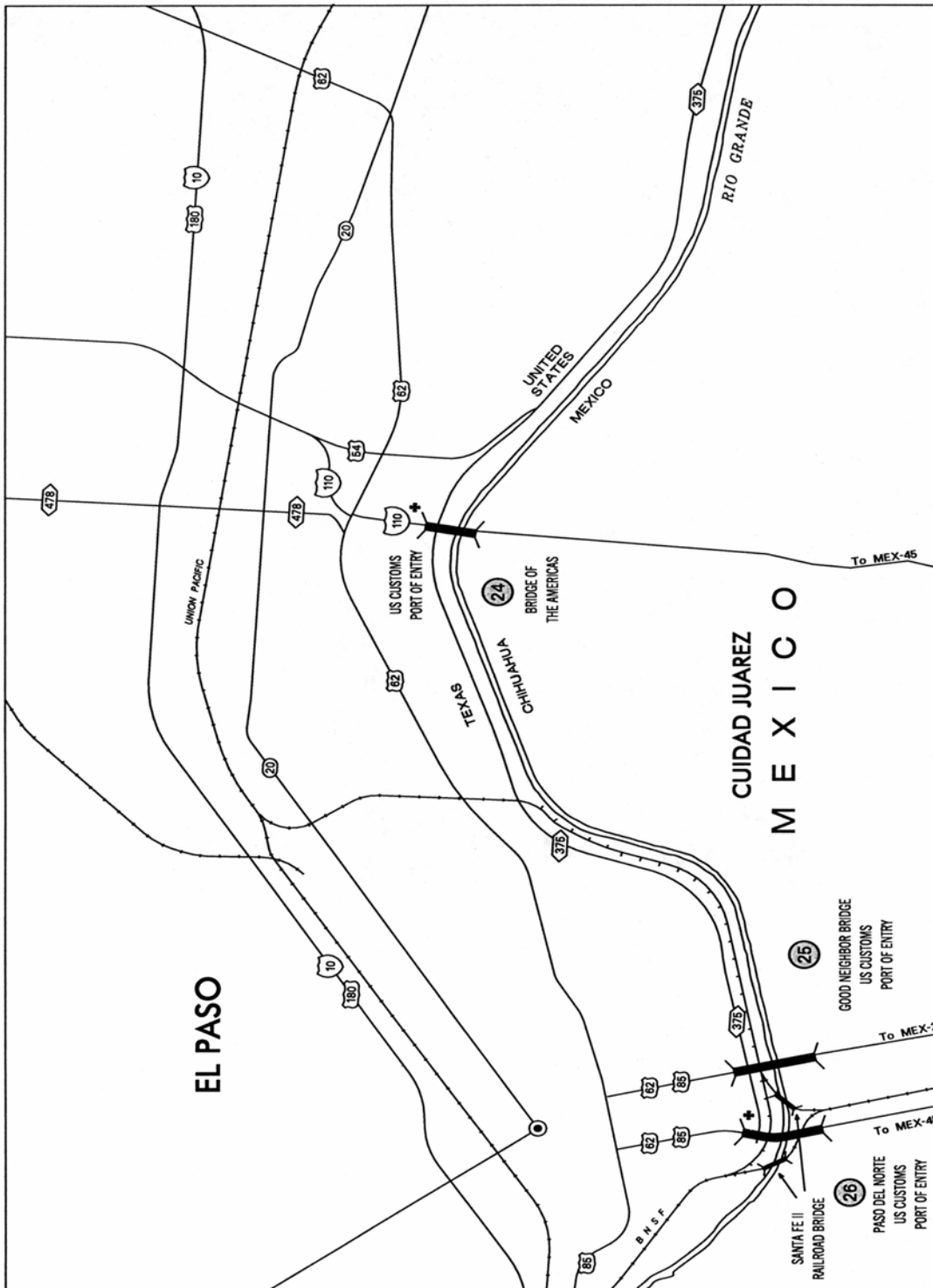


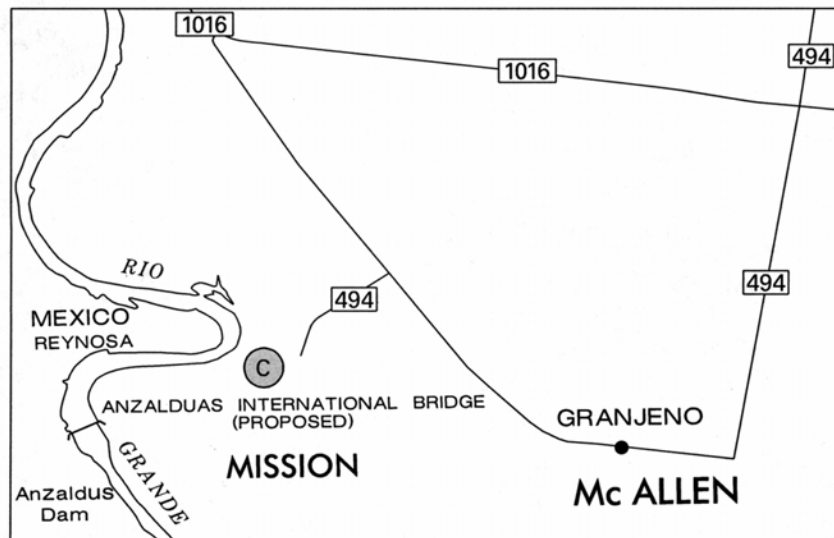
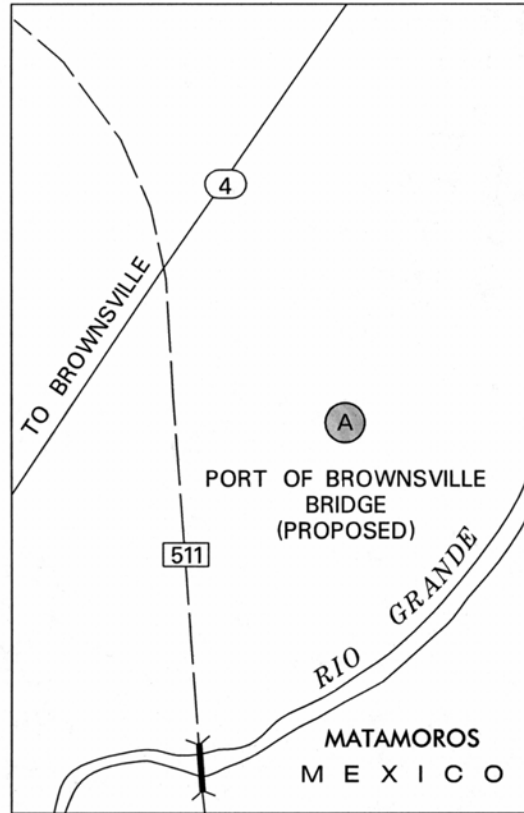


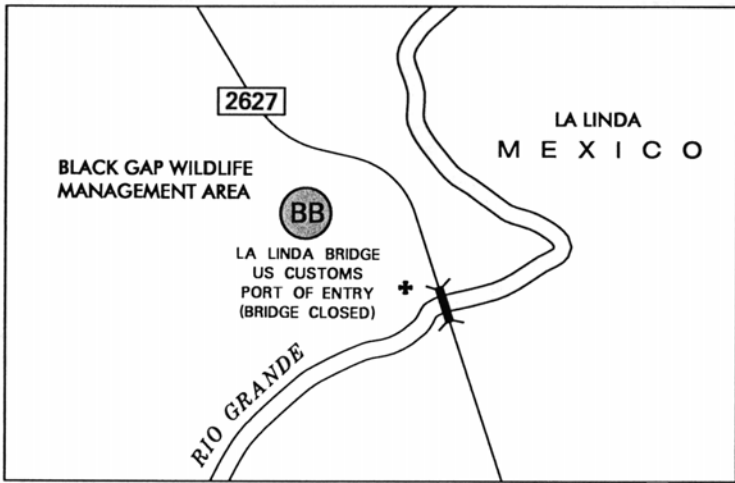
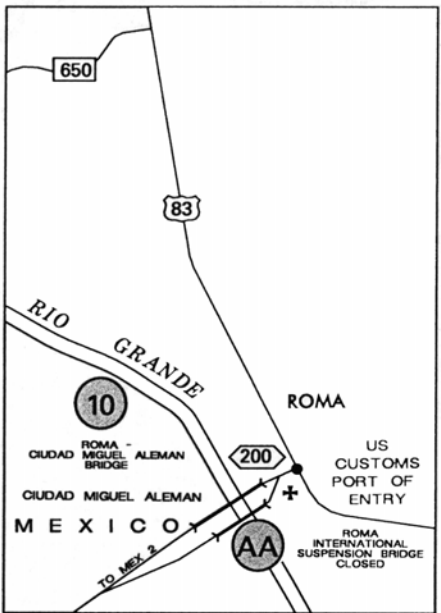












Appendix VI

Glossary

GLOSSARY OF SELECTED TERMS USED IN THIS REPORT

B

BCBP – U.S. Bureau of Customs and Border Protection – Formerly the border protection and inspection functions of the Immigration and Naturalization Service (INS), Border Patrol, Customs and Animal Plant Health Inspection Services (APHIS). BCBP became an official agency of the U.S. Department of Homeland Security on March 1, 2003.

BND -- Brownsville Navigation District/Port of Brownsville -- The port authority responsible for managing the deep-water port and industrial park.

BRINSAP -- Bridge Inventory, Inspection and Appraisal Program -- The group responsible for monitoring and overseeing the federally mandated Bridge Inspection Program.

Border Station Task Force -- This group is chaired by the General Services Administration Region 7, and consists primarily of various regional representatives from the federal inspection services. Some Texas State agencies, like TxDOT, are also invited to attend. Federal representatives include the U.S. Customs and Border Protection, U.S. Department of Agriculture, U.S. Immigration and Naturalization Service, International Boundary and Water Commission and the U.S. State Department. The task force meets one or two times a year to discuss border station development and maintenance issues in Texas and New Mexico.

C

CABIN/SECODAM – Comisión de Avalúos y Bienes Naturales/Secretaría de Contraloría y Desarrollo Administrativo – Mexico's counterpart of the GSA.

CAPUFE - Caminos y Puentes Federales de Ingresos y Servicios Conexos -- The Mexican federal toll-road and federal toll-bridge operator. CAPUFE collects tolls on international bridges, and operates and administers most bridge operations on the Mexican side.

CBI Program – Coordinated Border Infrastructure Program -- An FHWA program under which border states and MPOs are eligible for discretionary grants for transportation and safety infrastructure improvements, operation and regulatory improvements; and coordination and safety inspection improvements in a border region.

CILA – Comisión Internacional de Limites y Aguas -- The division of the SRE that oversees the boundaries for rivers and border waters. Mexico's counterpart of the IBWC.

D

DCL – Dedicated Commuter Lane – A lane used exclusively for commuter traffic.

F

FIS -- Federal Inspection Services -- Consists of the main federal inspection services present at each port of entry, i.e., Customs and Border Protection, and USDA.

FONSI - Finding of No Significant Impact -- This process is related with the application for a Presidential Permit and is issued by the State Department. This finding considers the environmental impact, whether direct, indirect, or cumulative, in relation to the proposed facilities and related construction of the proposed bridge site. FONSI are given out by either the FHWA or TxDOT depending on whether the project is state or federally funded.

FHWA – Federal Highway Administration – A component of the U.S. Department of Transportation, established to ensure development of an effective national road and highway transportation system. It assists States in constructing highways and roads and provides financial aid at the local level, including joint administration with the Federal Transit Administration of the Section 18 program.

Source: *Glossary of Transportation Terms 1994/Glosario de Términos Transportes 1994*, U.S. Department of Transportation, Federal Highway Administration

G

GSA --General Services Administration -- The U.S. federal agency whose responsibilities include design, construction and maintenance of border station facilities leased to federal inspection services.

General Services Administration Southwest Border Station Improvement Program, January 1, 1994 --

An inventory of IMPROVEMENTS for existing border stations, as well as appropriations, along the U.S.-Mexican border. Published by the General Services Administration.

H

HAZMAT – Hazardous Material

I

IBWC -- International Boundary and Water Commission, United States and Mexico -- A joint U.S.-Mexican commission that is charged with resolving problems relating to border water issues and to the sovereignty of lands incidental to changes in the courses of river boundaries.

INM - Instituto Nacional de Migración -- A division of SEGOB responsible for immigration, the Mexican counterpart of the INS.

INS -- Immigration and Naturalization Service -- The U.S. federal agency formerly responsible for enforcement of immigration law. These functions are now part of the U.S. Bureau of Customs and Border Protection and an agency of the U.S. Department of Homeland Security.

M

MPO – Metropolitan Planning Organization – An organization designated by the governor to administer the federally required transportation planning process in a metropolitan area. An MPO must be in place in every urbanized area over 50,000 population.

N

NAFTA – North American Free Trade Agreement

P

POV – Privately Owned Vehicle

PS&E – Plans, Specifications and Estimates -- The detailed plans and accompanying specifications and construction cost estimates which serve as documents for construction contract letting purposes.

Presidential Permit -- The first step in the federal permit process for a U.S. sponsor of a proposed bridge to begin construction. By presidential delegation, the Presidential Permit is issued by the U.S. State Department. No Presidential Permit is issued in Mexico although similar final approval is given by the federal government to the Mexican sponsor to begin construction.

PROFEPA – Procuraduría Federal de Protección al Ambiente – Federal Bureau of Environmental Protection

Programa Nacional de Autopistas 1989-1994, Propósitos y Logros -- A book published by the Secretaría de Comunicaciones y Transportes that highlights selected highway projects in Mexico. Information contained in the book includes photographs, descriptions of projects, costs and information about the owner or concessionaire. Published in July 1994.

Promofront S.A. de C.V. -- A Mexican construction company.

S

SAGAR - Secretaría de Agricultura y Ganadería -- Responsible for inspecting imported ranching and agricultural products. The Mexican counterpart of the U.S. Department of Agriculture.

SCT - Secretaría de Comunicaciones y Transportes -- Ministry of Communications and Transportation. The Mexican federal agency responsible for construction, operation and maintenance of the federal highway system, including federal toll roads and bridges. Mexico's counterpart to the U.S. Department of Transportation.

SECODAM /CABIN -- Secretaría de Contraloría y Desarrollo Administrativo/Comisión de Avalúos de Bienes Naturales. The Mexican counterpart of the GSA.

SECOFI - Secretaría de Comercio y Fomento Industrial -- Mexican federal agency that oversees NAFTA negotiations.

SECTUR - Secretaría de Turismo -- Mexico's Ministry of Tourism. Studies tourist information at some of the bridges and border crossings.

SEDENA -- Secretaría de la Defensa Nacional -- Mexican federal agency that authorizes locations for new bridges and border crossings.

SEDESOL -- Secretaría de Desarrollo Social -- Mexican federal agency responsible for urban planning in border cities.

SEDICOT - Secretaría de Desarrollo Industrial, Comercial y Turístico -- The Tamaulipas state agency responsible for industrial, commercial and tourism development.

SEGOB -- Secretaría de Gobernación -- Ministry of Government

SEMARNAP -- Secretaría del Medio Ambiente Recursos Naturales y Pesca -- Authorizes Environmental Impact Studies. The Mexican counterpart of the EPA.

SENTRI -- Secure Electronic Network for Travelers' Rapid Inspection

SHCP -- Secretaría de Hacienda y Crédito Público -- One of its offices (Administración General de Aduanas) is responsible for controlling and reviewing merchandise that is imported and exported. The Mexican counterpart of the U.S. Customs.

SIB Loan -- State Infrastructure Bank Loan

SRE - Secretaría de Relaciones Exteriores -- The Ministry of Foreign Relations. The Mexican counterpart of the U. S. State Department.

Summary of Existing and Proposed Border Stations, May 1994 -- An inventory of existing and proposed border stations in Texas and New Mexico, with general information and traffic data. Published by the General Services Administration.

T

TABC – Texas Alcoholic Beverage Commission

TNRCC – Texas Natural Resource Conservation Commission, now known as the Texas Commission on Environmental Quality (**TCEQ**).

TPP – The Transportation Planning and Programming Division of TxDOT

TxDOT -- Texas Department of Transportation

TxDOT District -- The State of Texas is divided into 25 TxDOT districts

U

USCG – United States Coast Guard. Under the U.S. Department of Homeland Security

USDA – United States Department of Agriculture

U.S. Coast Guard Permit -- The Coast Guard's authority regarding international bridges stems from the International Bridge Act of 1972. Under the provisions of the Act, the Coast Guard has jurisdiction pertaining to the construction, operation and maintenance of any bridge connecting the United States with a foreign country.

FOR FURTHER INFORMATION

Additional information regarding border crossings in Texas can be obtained from the following:

International Relations Office
Texas Department of Transportation
4000 Jackson Ave.
Austin, TX 78779-001
(512) 374-5326

Transportation Planning and Programming Division
Texas Department of Transportation
118 E. Riverside Dr.
Austin, TX 78701
(512) 486-5000

Mr. Chuck Berry, P.E.
District Engineer, El Paso District
Texas Department of Transportation
13301 Gateway Blvd. West
El Paso, TX 79928-5410
(915) 790-4200

Mr. Luis A. Ramírez, P.E.
District Engineer, Laredo District
Texas Department of Transportation
1817 Bob Bullock Loop
Laredo, TX 78043
(956) 712-7400

Mr. Mario Jorge, P.E.
District Engineer, Pharr District
Texas Department of Transportation
P.O. Box 1717
Pharr, TX 78577-1717
(956) 702-6101

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Attn: Manuela A. Ortiz
International Relations Office-TxDOT
4000 Jackson Ave.
Austin, TX 78779-001
Phone (512) 374-5326
Fax (512) 374-5325